

Access to Single Houses and Small Housing Developments Inntrigeadh gu Taighean Singilte agus Leasachaidhean Beaga Taigheadais



Guidance on Access, Road Safety and Drainage Requirements for Single Houses and Small Residential Developments of up to 4 Houses where the Developer proposes to construct a Private Access connected to the Public Road Network.

Stiùireadh mu Inntrigeadh, Sàbhailteachd Rathaid agus Riatanasan Drèanaidh airson Taighean Singilte agus Leasachaidhean Beaga Taigheadais le suas ri 4 Taighean far a bheil an Leasaiche a' moladh Inntrigeadh Priobhaideach a thogail a tha ceangailte ri Lìonradh Rathaidean Poblach

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1.0 INTRODUCTION

RO-RÀDH



- The aim of this document, on the construction of private accesses, is to offer guidance to small developers who propose to take access off the public road (excluding trunk roads). For guidance on other planning issues please refer to the relevant Council publications.
- We recommend that at an early stage you employ the services of a Professional Architect, or Engineer, to assist you with your project.
- A 'Private Access Checklist' is attached to this booklet and developers should complete this checklist and enclose a copy with their planning application. This will assist in a prompt consideration of the planning application.
- Completion of the checklist will help highlight, at an early stage, any potential problems to the Council, assist in processing the application, and identify if there is a need for further information or consultation.
- When there is a possibility, that in future, a rural development will be extended to more than four houses then the access road will require to be constructed to an adoptable standard and advice should be sought from the Council.
- In an Urban Area, extending an existing public road, will require the extension to be constructed to an adoptable standard and advice should be sought from the Council.
- **Note** – If the access to the proposed development is from a Trunk Road you will need to consult the Council as different standards may be required.

2.0 OBJECTIVES

AMASAN



The Highland Council wishes to promote consistent standards for the construction of private accesses where they connect to the public road network.

The Main Objectives are:-

- **Safety**
 - The Creation of Good Access Visibility.
 - The Provision of Service Bays where appropriate.
 - Spacing from existing Junctions and Accesses.

- **Drainage**
 - Provision of adequate Roadside Drainage.
 - Site Water Run Off correctly disposed off.
 - Flood Risk minimised.

- **Construction**
 - Consistent Design.
 - Acceptable Construction Standards.
 - Compliance with Legislation.

3.0 SITE ASSESSMENT MEASADH LÀRAICH



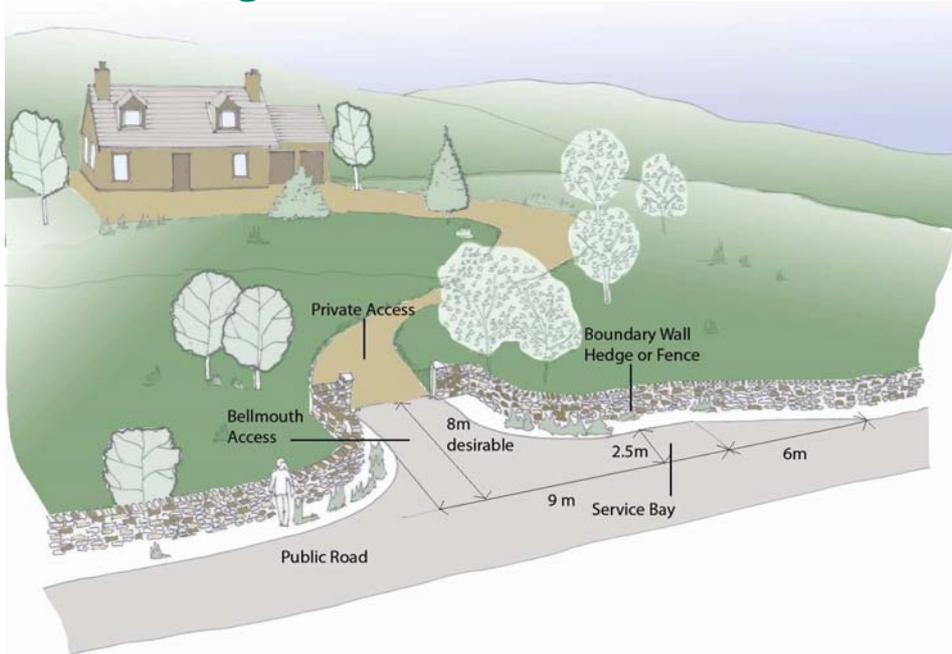
When considering the location of your new access The Highland Council would like you to consider the following:-

- Is the proposed access in the best location?
- Can I share an access with a neighbour?
- Is the proposed access close to a road junction or other property access?
- Is the proposed access close to a blind bend or crest in the road?
- Can I construct a safe access visible to other road users?
- Have I control over all the land required for any necessary improvements?
- Do I need to set back boundary walls or fences?
- Do neighbouring properties obstruct visibility?
- Is the site large enough for the provision of car parking and turning?
- Do I need to extend the existing footpath to join my development?
- Is the site liable to flooding or have drainage problems?
- Are there underground services in the footpath or verge?
- Are there overhead services which could be hazardous?
- Do I require permission to excavate in the road, footpath or verge?
- Are there listed structures or protected species or trees likely to affect visibility?
- Do I need to provide a storage area for refuse and recycling bins?

The above list is not exhaustive but guidance is given in the following sections of the Booklet, on what the required Standards are, and how they can be achieved.

4.0 DESIGN GUIDANCE - RURAL ROADS STIÙIREADH DEALBHAI DH – RATHAIDEAN DÙTHCHAIL

4.1. Service Bay & Access Layout Bàgh Sheirbheisean & Coltas An Inntrigidh



4.1.1 A private access, to be constructed on a rural road, will require a combined Service Bay and Access to be incorporated in the layout. Service Bays are required, so that on completion of the development, delivery vehicles, postal services and refuse vehicles can stop clear of the carriageway without causing an obstruction to the free flow of other vehicles on the public road. The Service Bay must be constructed prior to commencing work on the development, as the service bay will assist in the safe delivery of building materials to the site, and allow contractors vehicles to manoeuvre and park temporarily. See Drawings No SDB1 and No SDB 2 in Appendix 1 for dimensions and layouts.

4.1.2 The service bay must have a final layer of bituminous surfacing and this must also extend up the access road for a distance of at least 6 metres from the public road edge in order to prevent gravel from the private access, being dragged onto the public road.



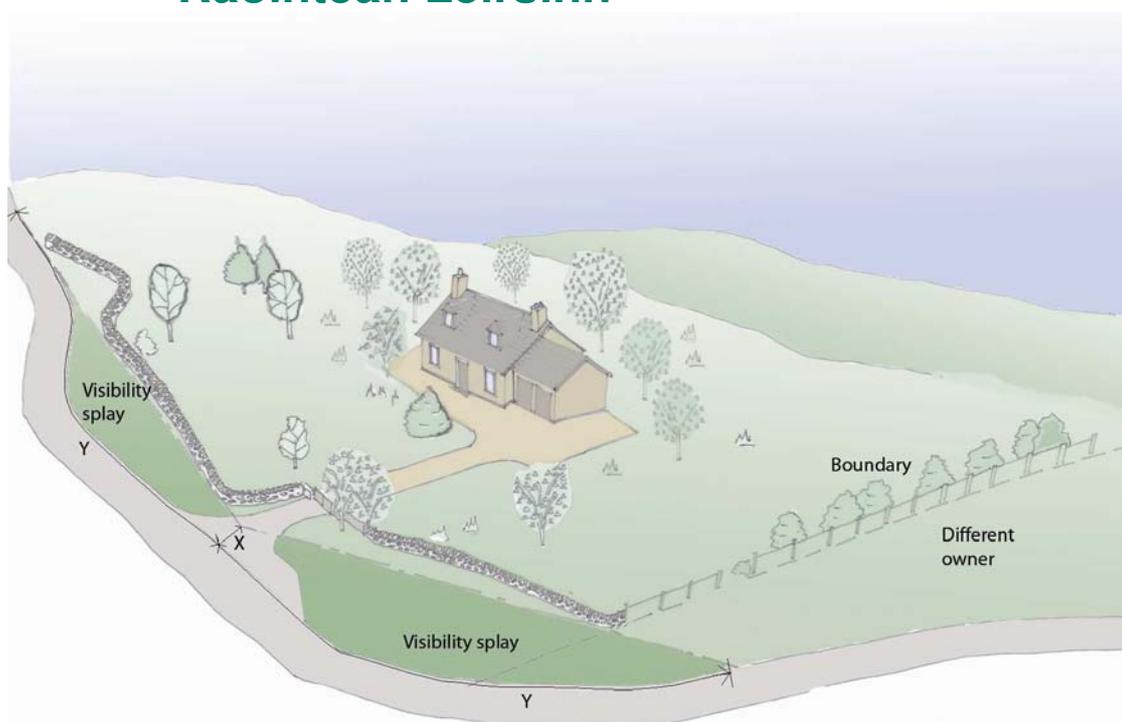
Typical Service Bay

4.1.3 When the site is on a hillside, either above or below the road, the slope of the access, after a gentle slope for the first 5 metres from the public road, should normally be no greater than 1 in 10 thereafter.

4.1.4 In the interests of road safety the Council wishes to encourage the use of shared accesses where possible. Any new private access should not normally be situated within 90 metres of a road junction or within 30 metres of an existing property access. These distances are appropriate for most minor rural roads but on higher speed Class A & B roads, a greater separation distance between accesses may be required where an increase in the number of accesses could affect traffic safety and speed. It is recommended that on Class A & B rural roads, the Council be consulted for advice, prior to submitting your application.

4.2. Visibility Splays

Raointean Lèirsinn



4.2.1 A Visibility Splay is an essential feature of your access and service bay provision.

4.2.2 What is a Visibility Splay? In the sketch above, imagine you are leaving the property about to join the public road, you look to the right and left across the verge and land adjacent to the road, to see if there is any approaching traffic. The areas shown by the darker shaded areas are called the Visibility Splays.

4.2.3 The purpose of Visibility Splays is also to allow traffic on the public road to see, cyclists, vehicles and pedestrians leaving the development. An unobstructed visibility splay allows you to SEE and be SEEN, therefore the longer the length of visibility splay, the more time another road user has to see you, and for you or your family to see them, and react to any potential incident.

4.2.4 How is it measured? There are two key dimensions shown as X and Y in the sketch above. Firstly, the fixed X dimension 2.4 metres long, is taken from the edge of the carriageway back to the driver's eye line, which for an average car, is 1.05 metres above the road.

4.2.5 Secondly, the distance you need to see along the road edge (the Y distance) is measured, at the height of the driver's eye line, from the centre line of the access to the location on the road of the approaching vehicle, which varies depending on the speed of approaching traffic. The faster the approaching vehicles, the longer the distance required to see and be seen. The two figures X and Y are used in Table 4.3.1 to calculate the visibility splay for any individual access.

4.2.6 The access, service bay and visibility splay must be established before building work commences, as your builders and tradesmen are entitled to a safe access as well.

Note – The legal procedures necessary to carry out the works are described in sections 6.0 and 6.1.



Typical Visibility Splay from the Driver's Eye view

4.3 Obtaining the required Visibility Splays

A' Faighinn nan Raointean Lèirsinn riatanach

4.3.1 The visibility splay is obtained by measuring 2.4 metres from the edge of the carriageway along the centre line of your access (the X figure in the sketch in Section 4.2) and taking the Y figure from Table 4.3.1 below. The Y distance is measured from the centre line of your access along the edge of the public road as shown in section 4.2 and in the sketches 4.3.1 and 4.4.2 below.

The traffic speeds to be used in the calculation of the Y distance are normally the speed limits which apply to the public road your access will connect to.

4.3.2 On some roads, such as single track, or minor roads where traffic speeds can be significantly lower, the developer may estimate the speed of traffic passing the site, by conducting a practical drive past the site assessment, or in the case of disagreement, by means of a professional traffic speed survey. The estimated speed, if lower than the speed limit, will require to be agreed by the Council and should be highlighted in your planning application and in the checklist to be found in Appendix 2 of this booklet.

SPEED MPH	30	40	50	60
'Y' Distance in metres	90	120	160	215
'X' Distance in metres	2.4			

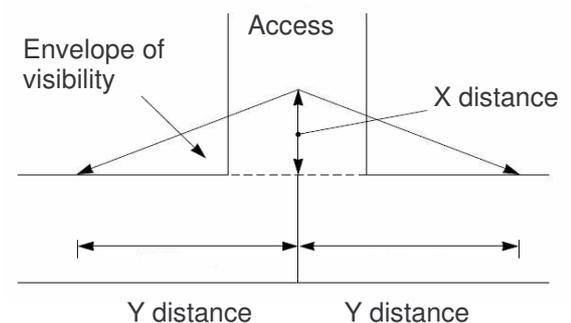


Table 4.3.1

Sketch 4.3.1

4.4 Providing and Maintaining Visibility Splays

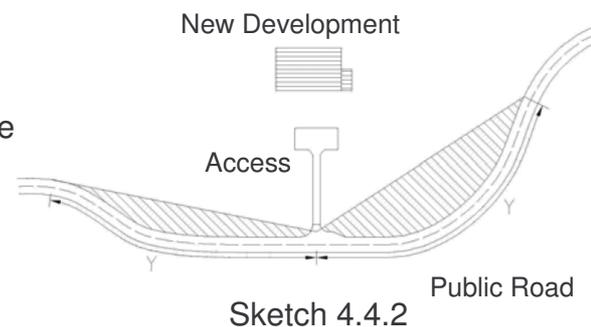
A' Solarachadh is a' Cumail Suas Raointean Lèirsinn

4.4.1 When submitting your planning application you must also demonstrate that you have, and can maintain control over your visibility splay area. For your own safety you will have responsibility for the maintenance of clear sightlines over the visibility splay area. If the visibility splay intrudes into your neighbour's land you will need to discuss this with your neighbour and the Council who will advise you on any legal agreements which may be needed

4.4.2 There are circumstances when the developer wishes to locate the access on or near a bend in the road. The outside of a bend is the safest option, however there may be no alternative but to locate the access on the inside of a bend, and therefore the developer should be aware of the area of the site which will be affected by the visibility requirements, and which must be kept free of obstructions such as buildings, trees and tall shrubs.

Note - Where visibility is obstructed by listed buildings, walls, or protected trees you should consult the Council for advice.

4.4.3 The achievable visibility splays should be dimensioned and shown on your site plan.



4.5 Parking and Turning of Vehicles

Parcadh agus Tionndadh Charbadan

4.5.1 The provision of a minimum of two parking spaces within the curtilage of the property is the normal requirement for rural developments. However, an equally important requirement is the provision of an adequate turning area, as vehicles which enter the development **MUST** be able to park and/or turn round, and leave the site in forward gear.

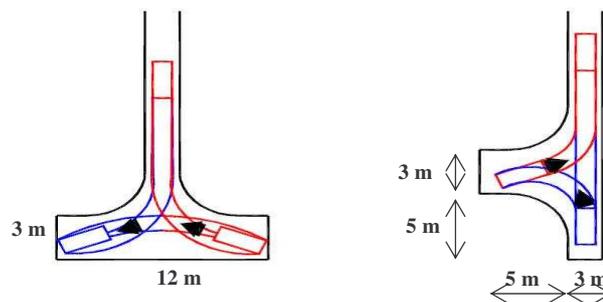
This rule applies, whether you are developing a single house site, or a site with several properties.

Vehicles must not reverse out onto the public road. Developers should consider how materials such as heating oil are delivered and for example place their fuel tank where it can be accessed from the service bay.

Where there is more than one house this may not be a practical option, then sufficient turning area must be provided within the development site to enable the delivery vehicle to turn round.

Note - Areas provided for turning are in addition to those provided for parking.

4.5.2 The sketches below are guides to possible layouts for turning areas



Many areas of the Highlands can experience severe winter weather and for those developments where the access road is particularly steep, consideration should be given to providing, if land is available, parking areas at the top/bottom of the incline.

4.6 Access Gates

Geataichean Inntigidh

4.6.1 Access gates, if provided should normally be located at least 8 metres from the adjacent public road edge to ensure vehicles waiting to enter or leave the access do not obstruct the public road while the gates are being opened or closed. Gates must open away from the public road. The figure of 8 metres allows a vehicle with a trailer to stop clear of the public road. Gate pillars, walls, fences or hedges must be kept to the rear of the visibility splay area, or if that is not possible, must not obstruct the sight lines. Cattle grids if provided must be located out with the public road boundary.

In rural areas where properties may be relatively remote from the public road and emergency vehicles may therefore have to enter the curtilage of properties, access



drives shall be 3.3 metres minimum width with 3.5 metres minimum clearance between gate posts.

4.7 Roadside Drainage

Drèanadh Ri Taobh Rathaidean

4.7.1 Any existing roadside ditch should be taken, where possible, to the rear of the service bay and under the access by a pipe, minimum diameter of 300mm.

The applicant should consult the Council, or inspect adjacent pipes or culverts to ascertain whether a greater size of pipe has been deemed necessary by local farmers, who will have previous knowledge of flood conditions in the area, and increase the size of pipe if necessary. Deep and wide ditches may require larger diameter pipes. As a general rule, the bigger the pipe, the lower the risk of blockage, so go up a size rather than down.

Prior to laying the pipe, the ditch should be cleaned out along the frontage as necessary, the pipe placed on, and surrounded by concrete, 150mm thick, if required by the Council.



Drainage Headwall

4.7.2 Headwalls of masonry or concrete should be provided, so the end of the pipe can be located, should the pipe become blocked during flood conditions.

4.7.3 The final bitumen macadam surface of the service bay must be shaped to prevent water, from the new access, flowing onto the public road. This is to prevent wet areas, which could freeze in winter, and the developer should also take steps to ensure water flowing along the public road does not, and cannot enter the site, and give rise to flooding problems. Methods of achieving this are shown in Drawings SDB 4 & 5 in Appendix 1

4.7.4 On many rural roads, there are no ditches and roadside drainage is by offlets or grips, (the small ditches cut into and across a verge) which discharge water into adjacent land. The Developer must inspect the site, and where offlets currently discharge into the site, must accept that water and make provision to deal with, and dispose of it.



Typical Drainage Offlet

4.8 Dealing with Storm Water

A' Dèiligeadh ri Uisge Stoirm

4.8.1 During heavy rain, water from farmers fields can flow onto the road, culverts become choked, and roadside ditches overtop with the result that significant water flows along the public road. Thankfully these events are rare, but they do occur and developers should take measures to prevent water flowing into their development. The simplest solution to deal with this water is to ensure that the access slopes upwards for a short distance, forcing the flood water to continue along the road edge, until it finds its natural escape route.

4.8.2 Where water can enter the site, ditches or a piped drainage system should be installed within the site and water re-directed back into the natural drainage system.



4.9 Potential Flooding Risk Factors

Nithean a Dh'fhaodadh Tuil Adhbhrachadh

4.9.1 In order to avoid building on sites susceptible to flooding, the following risk factors should be considered.

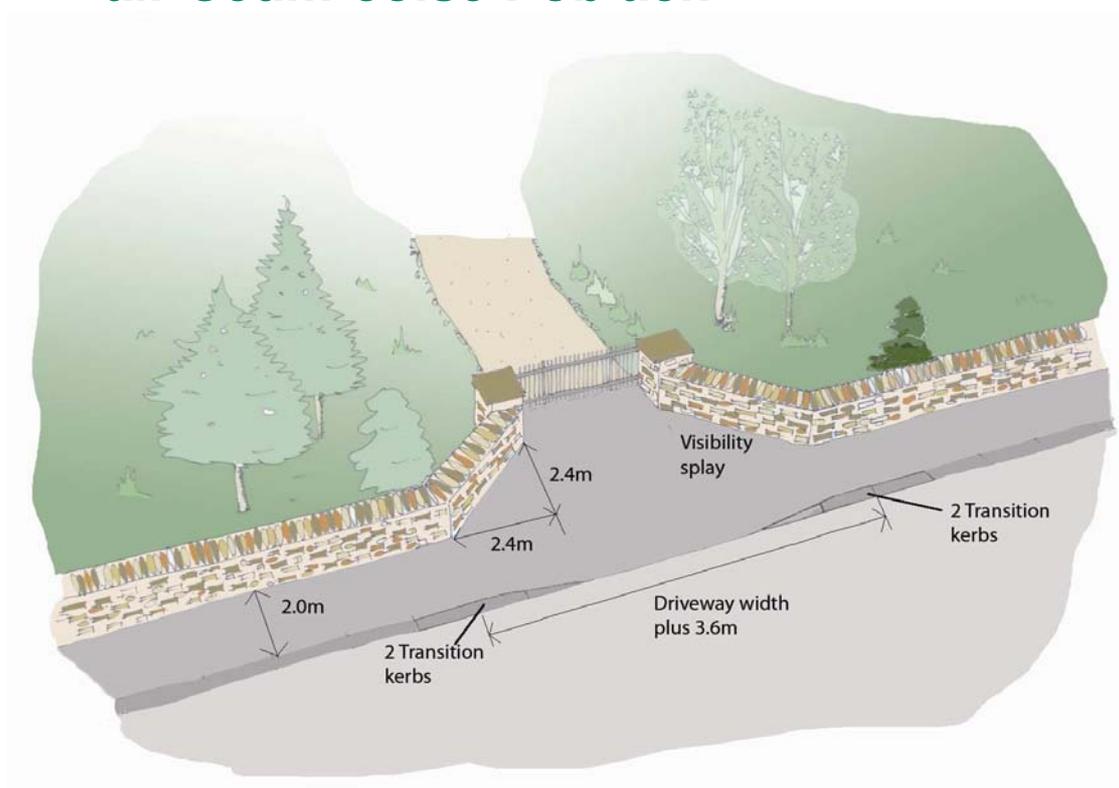
- 1) A roadside ditch with significant water flow or signs of flow.
- 2) Adjacent streams, burns or ditches which could overtop in storm conditions.
- 3) Is the site located in a natural basin, in which water could accumulate?
- 4) Are there any underground springs discharging water on the site?
- 5) Any natural gullies which could direct water run-off towards the site?
- 6) Higher ground above/behind which may require the installation of cut off drains?
- 7) The proximity of the site to a river (erosion and/or flooding).
- 8) The height of a shoreline site in relation to High Tide Level (MHWS).

4.9.2 It may also be worth examining the SEPA Flood Map at www.sepa.org.uk/flooding. However it should be noted that this relates to large scale flood risk assessment and does not necessarily show sites that may be susceptible to localised flooding. Sites which may be at risk of flooding will require a flood risk assessment to be undertaken by a suitable specialist and you should consult the Council for advice on this aspect.

5.0 DESIGN GUIDANCE – EXISTING RESIDENTIAL AREAS

STIÙIREADH DEALBHÀIDH SGÌREAN CÒMHNAIDH A THA ANN MAR-THÀ

5.1 Formation of a Vehicular Access across a Public Footway Cruthachadh Inntrigeadh Charbadan thairis air Ceum-coise Poblach



5.1.1 This section of the booklet is aimed at the Developer, who may be constructing a single house, or extending a property, and wishes to construct a new private access, or improve an existing access in a residential area.

5.1.2 Developments in or adjacent to existing residential areas, should be assessed for access by walking and cycling, and may require links to footpaths, and to other facilities such as bus services, or schools. The Developer is therefore encouraged, to seek, early consultation with the Council, to discuss these issues.

5.1.3 A development in an existing residential area, will normally involve taking access across a footway, safety of pedestrians is the highest priority, and as pedestrians can be young, elderly, or visually impaired, it is essential that driver's using the access, have an unobstructed view of footway users.

5.1.4 Kerbing and footway levels will normally require to be adjusted and a qualified roadwork contractor should be engaged to carry out this work.

Note that the footway should be kept level for the greater part of the crossing for ease of pedestrian movement with the drop for vehicle access restricted to a zone near the kerb edge. See Drawing No SDB 3 in Appendix 1 for details.

Note – The legal procedures necessary to carry out the work are described in section 6.0 Legal Responsibilities and in section 6.1 Road Opening Permits.

5.2 Obtaining the required Visibility Splays

A'Faighinn nan Raointean Lèirsinn riatanach

5.2.1 The Council requires that visibility splays should be provided in accordance with the same principles as described in Section 4.2 to the dimensions shown in Table 5.2.1 below.

5.2.2 On residential roads an X distance of 2.4 metres is the standard distance from the edge of the carriageway to the driver's eye line.(See Section 4.2.4)

5.2.3 When establishing the required horizontal sight line distance Y use the appropriate speed limit which applies to the road your access connects to.

Speed M.P.H.	'X' Distance in Metres	'Y' Distance in Metres
20	2.4	25 (Minor Residential Roads)
30	2.4	40 (Minor Residential Roads)
30	2.4	90 (General Residential Roads)
40	2.4	120 (Distributor Roads)

Table 5.2.1

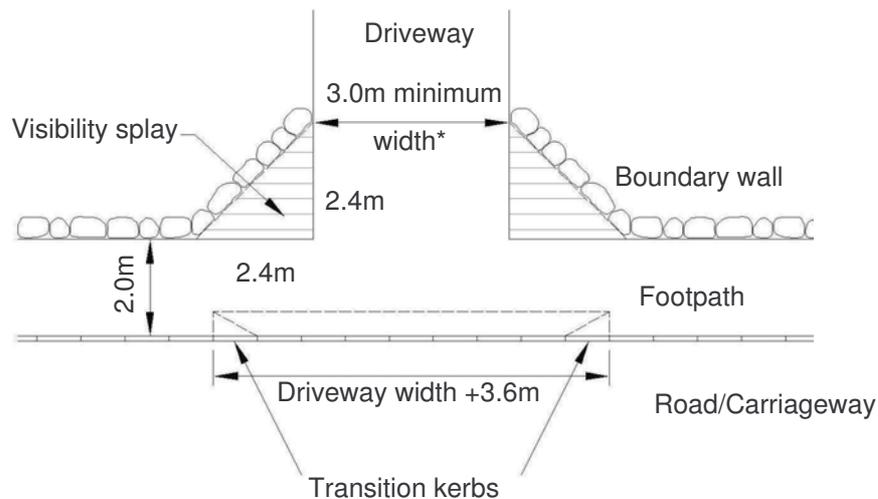
5.3 Providing and Maintaining Visibility Splays

A' Solarachadh is à Cumail Suas Raointean Lèirsinn

5.3.1 The visibility splay can be achieved by ensuring walls, fences, or hedges within the area of the visibility splay are no higher than 0.85 metres, so that the vehicle driver leaving the property can see clearly, over the top of the wall, fence, or hedge and observe a child walking on the footway.

5.3.2 When submitting your Planning Application you must also demonstrate that you have and can maintain control over your visibility splay area. For your own safety you will have responsibility for the maintenance of clear sightlines over the visibility splay area. If the visibility splay intrudes into your neighbours land you will need to discuss this with your neighbour and the Council who will advise you on any legal agreements which may be needed. (See drawing at section 4.2)

5.3.3 In addition, if a high wall or fence is constructed or the existing wall or fence cannot be reduced in height or high hedges obstruct visibility, then 45 degree visibility splays as shown in the drawing below are required so that a pedestrian or cyclist can be seen.



*Note – A gateway width of 3.5 metres is required for emergency vehicles when door access to the property is more than 45 metres from the public road.

Sketch 5.3.1 Visibility Splay for Minor Residential Road

5.4 Dealing with Storm Water A' Dèiligeadh ri Uisge Stoirm

5.4.1 In Urban Areas, drainage gullies can become blocked with leaves or debris, leading to localised accumulation of water on the carriageway, but by ensuring that the footpath crossing slopes down towards the road and not the property, all but extreme flooding, should be prevented from affecting the development.



5.5 Parking Provisions Solar Parcaidh

5.5.1 In Urban Areas developers of single houses or extensions are normally expected to provide parking provisions for two vehicles within the site. The minimum area required for car parking is 6 metres long by 3 metres wide per vehicle, with the vehicles parked either end to end or side by side.

This parking provision is in addition to any garage which may be provided for parking.

Note – Where vehicular access is to be taken from a heavily trafficked road then turning provision within the site must also be provided so that vehicles can both leave and join the public road in forward gear. See section 4.5.2 for options.

5.6 Access Gates Geataichean Inntrigidh

5.6.1 Entrance gates may be set at the rear of the footpath as shown in photo 5.6.1

Gates must open away from the public road.

Note - the boundary wall lowered to provide good visibility over the adjacent garden.

5.6.2 Entrance gates can also be set at the rear of A low curved or recessed wall to provide good visibility
As shown in photograph 5.6.2 and 5.6.3



Photo 5.6.1



Photo 5.6.2



Photo 5.6.3

6.0 YOUR LEGAL RESPONSIBILITIES

NA DLEASTANASAN LAGHAIL AGAD

6.0.1 Obtaining Planning Permission for your new development is the first step in the legislative process, and further permissions are required under different and separate Roads legislation, as set out below, to enable you to create a private access, from a public road, or to adjust the existing access to your site.

6.0.2 Work in both Rural and Residential Locations should preferably be carried out by certificated contractors, whose employees have the necessary HAUC (Highway and Utilities Committee) certificates. Where the road, verge, or footpath contains apparatus from suppliers such as telecom, gas, water, and electricity (called statutory undertakers) these services must be protected, and HAUC certified personnel must be employed to carry out the work. The statutory undertakers (by legislation), require to be notified of any proposed excavation works via the Scottish Road Works Register. See 6.1 for information.

6.0.3 You or your contractor must obtain the appropriate permissions from the Council and provide the necessary information to enable the Council to input details of your proposed works on the Scottish Road Works Register.



Poor site access

6.0.4 When requesting connections to Gas, Electricity, and Telecom, these undertakers usually employ their own contractors, and will complete the required notices direct, but for Water & Sewerage connections you or your contractor are normally responsible for obtaining the appropriate consents from the Council and Scottish Water.

6.0.5 For your information and to ensure your Contractor is familiar with the up to date procedures, these are outlined in Sections 6.1 and 6.2.

6.0.6 Permits and further details are available from the Council.

6.1 Road Opening Permits

Ceadan Rathaidhean a Chladhach

6.1.1 You require a road opening permit, if you excavate in the road, verge, or footpath, for a new access, a water or sewerage connection, or intend to lay a buried cable or pipe across or along the public road. Statutory undertakers require a minimum of 10 days notice of the intended works, therefore you should request a Permit from the Council, at least 14 working days in advance of your proposed start date so that the Council can input the information on the Scottish Roadwork Register. Note - In exceptional circumstances up to 3 months notice is required.

6.1.2 Note – a fee is applicable for this permit and for subsequent inspections.

6.1.3 The legal framework for these road opening permits is Section 109 of the New Roads & Street Works Act 1991 and/or Section 56 of the Roads (Scotland) Act 1984

6.2 Occupation of the Road

Gabhail thairis an Rathaid

6.2.1 The requirements of this section are more likely to apply in the urban area, as in rural areas it will normally be expected that the road will be kept free of all obstructions. The Roads (Scotland) Act 1984 requires that permits are obtained for the following activities.

Permits Required

- To deposit a skip on the road.
- To deposit building materials on the road, footway, or verge.
- To erect scaffolding.
- To enable a crane to work on the road.

6.2.2 Your contractor will normally apply for these permits from the Roads Authority and pay the appropriate fees, but there may be circumstances, depending on how you manage the work, that you are obliged to do so.

- Remember to check, as you don't want your work stopped or delayed for lack of permits.

6.2.3 For Permits to undertake all of the above work, or for further advice, please contact the Council.

7.0 CONSTRUCTION TOGAIL

7.1 Signing & Safety Soighnichean & Sàbhailteachd

7.1.0 Once you have obtained information from the Statutory Undertakers on the location of their apparatus and the appropriate permit from the Roads Authority you are able to proceed with the excavation of the road, verge or footpath. You must ensure that the works are carried out in a safe manner, correctly signed and guarded to ensure the safety of the workers at the site and members of the public passing the works.

7.1.1 Your attention is drawn to the Code of Practice “Safety at Street Works and Road Works” issued by the Scottish Government and which is available on line at www.dft.gov.uk . Failure to comply with the code of practice may lead to criminal prosecution in addition to any civil proceedings.

7.2 Work Procedures Modhan Obrach

7.2.1 EXCAVATION Excavate carefully using hand tools to expose any undertakers apparatus, set aside topsoil for re use and remove material as required until the excavation is 240/350 mm below the existing footpath/carriageway respectively. If unsuitable material such as soil or peat is still visible then the unsuitable material should be removed and replaced with sound approved granular or crushed rock infill placed in 150mm layers and compacted with a twin drum or other approved compaction equipment. Before backfilling, take care to protect undertakers apparatus, as directed by them.

7.2.2 Sub Base, Binder Course, and Surface Course should be provided as shown in Drawings SDB 1, 2 & 3 in Appendix 1.

7.2.3 KERBING Where a drop kerb is to be installed on a footway, carefully remove the existing kerbs so that the existing kerb foundation can be re-used. Place the new kerbs as per the footway crossing specification shown in Drawing SDB 3 in Appendix 1.

7.2.4 VERGING Take the top soil previously set aside (or if necessary import material) and lay around the service bay and access to a nominal depth of 100mm and a minimum width of 0.5 metres. Seed as necessary.

Note – For further information and guidance, or for information on permitted alternative materials contact the Council.

Appendix No 1 Pàipear-taice Àir 1

DRAWINGS DEALBHANNAN

Drawing No SDB 1	Service Bay to a Rural Development with a Single House
Drawing No SDB 2	Service Bay to a Rural Development of up to 4 Houses
Drawing No SDB 3	Urban Footway Crossing
Drawing No SDB 4/5	Access/Carriageway Drainage Profiles

Note

When submitting your planning application, the above standard details, where relevant, must be incorporated in your architectural drawings/ plans.

Do **NOT** submit copies of these standard details on their own.

Applications with copies of the standard details attached will not be accepted.

To assist, the drawings above are available in electronic format for transferring to the applicants drawings.

TYPICAL MATERIAL SPECIFICATION



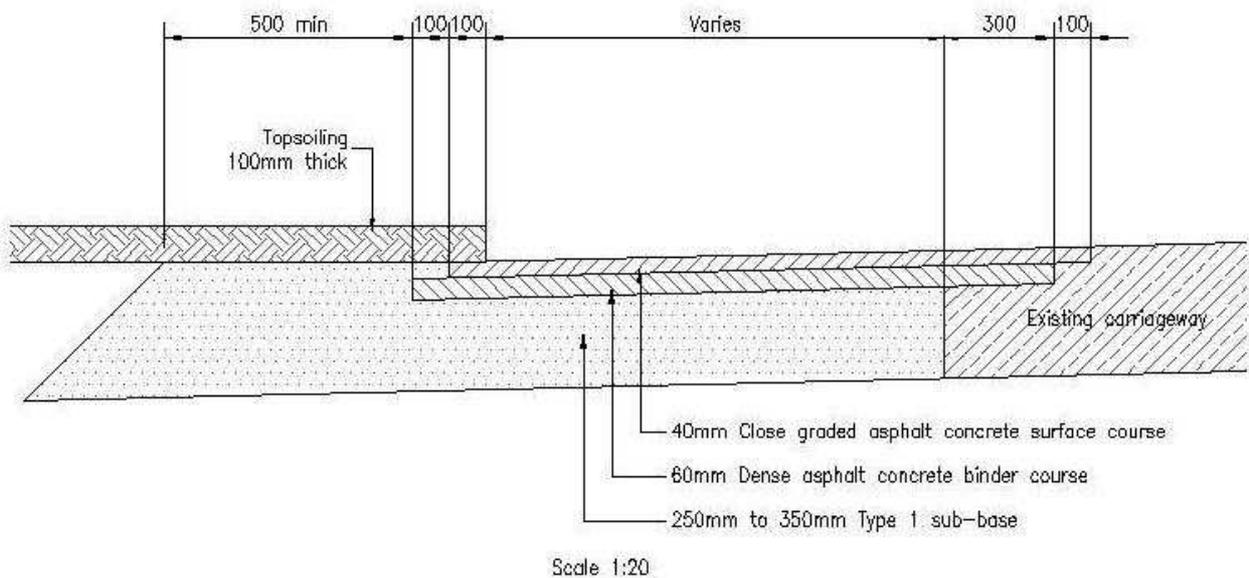
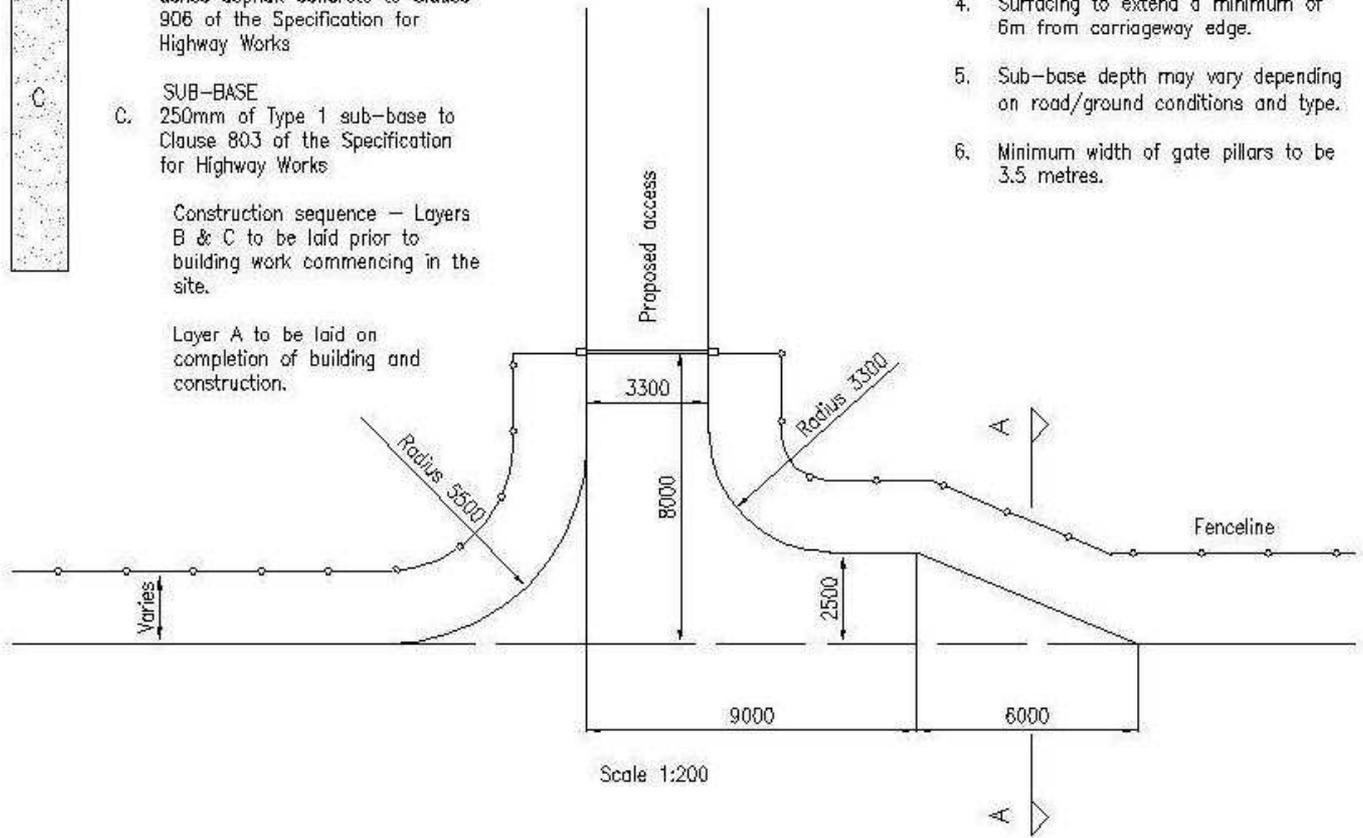
- SURFACE COURSE**
- A. 40mm of 14mm nominal size close graded asphalt concrete to Clause 912 of the Specification for Highway Works.
- BINDER COURSE**
- B. 60mm of 20mm nominal size dense asphalt concrete to Clause 906 of the Specification for Highway Works
- SUB-BASE**
- C. 250mm of Type 1 sub-base to Clause 803 of the Specification for Highway Works

Construction sequence – Layers B & C to be laid prior to building work commencing in the site.

Layer A to be laid on completion of building and construction.

Notes:

1. Verge a minimum of 500mm wide to rear of bituminous surface.
2. Service bay may be constructed on left or right hand side.
3. Gates should be set 8m from road edge and open inwards.
4. Surfacing to extend a minimum of 6m from carriageway edge.
5. Sub-base depth may vary depending on road/ground conditions and type.
6. Minimum width of gate pillars to be 3.5 metres.



PLAN AND SECTION OF SERVICE BAY TO A RURAL DEVELOPMENT WITH A SINGLE HOUSE

DRAWING No SDB 1

TYPICAL MATERIAL SPECIFICATION



SURFACE COURSE
 A. 40mm of 14mm nominal size close graded asphalt concrete to Clause 912 of the Specification for Highway Works.

BINDER COURSE
 B. 60mm of 20mm nominal size dense asphalt concrete to Clause 906 of the Specification for Highway Works

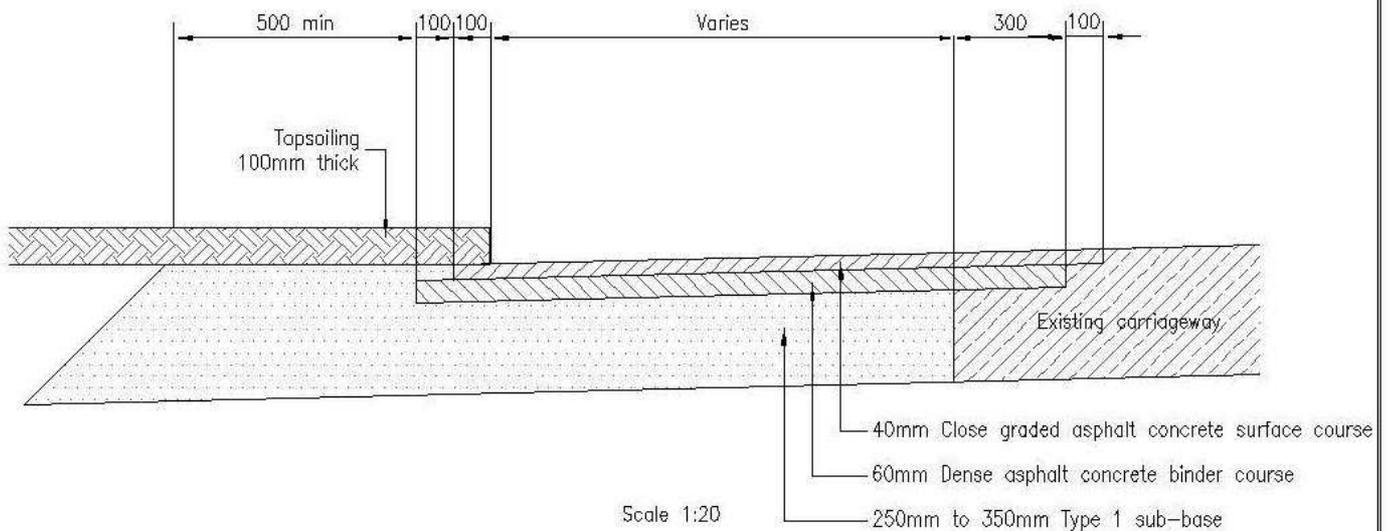
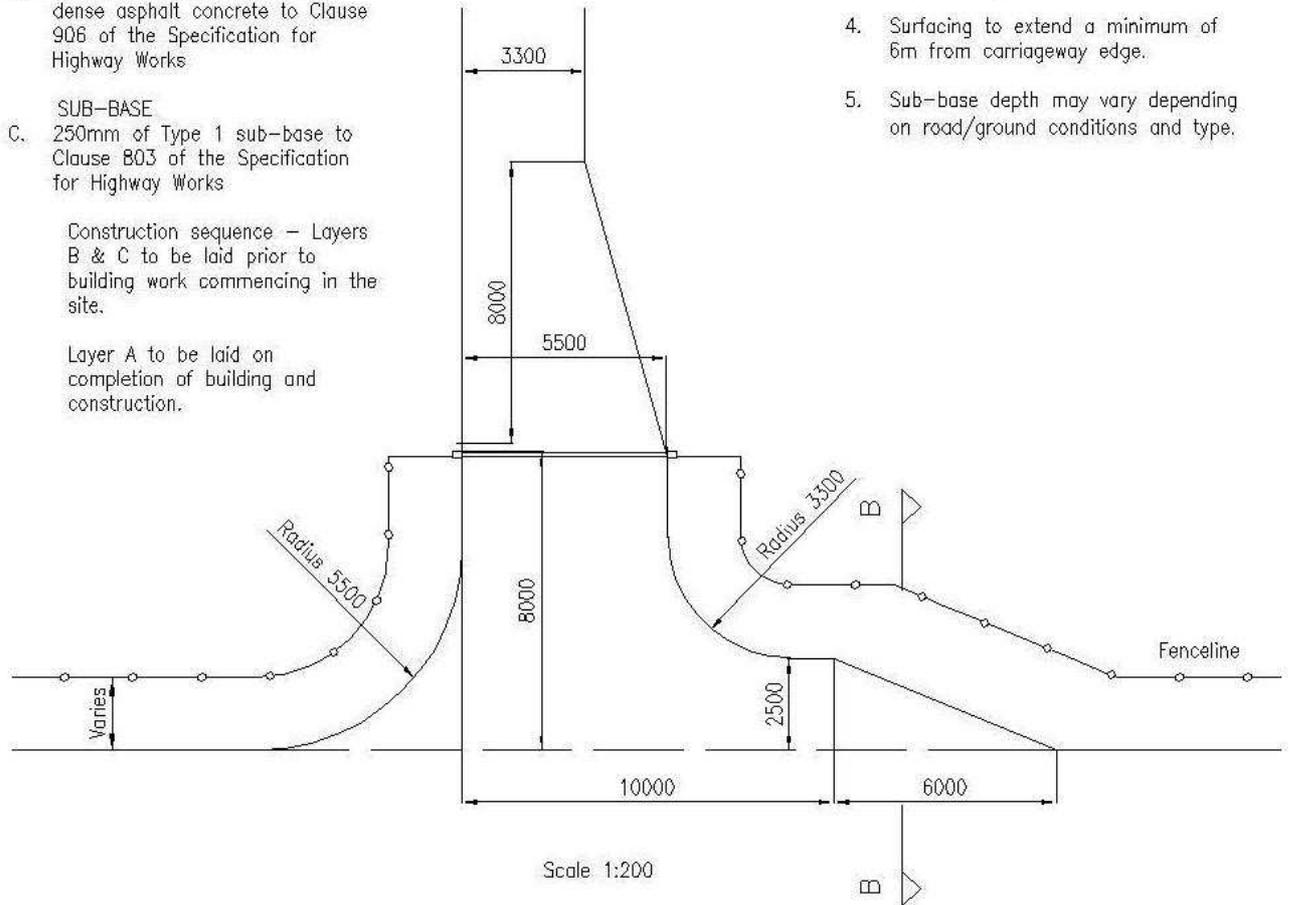
SUB-BASE
 C. 250mm of Type 1 sub-base to Clause 803 of the Specification for Highway Works

Construction sequence - Layers B & C to be laid prior to building work commencing in the site.

Layer A to be laid on completion of building and construction.

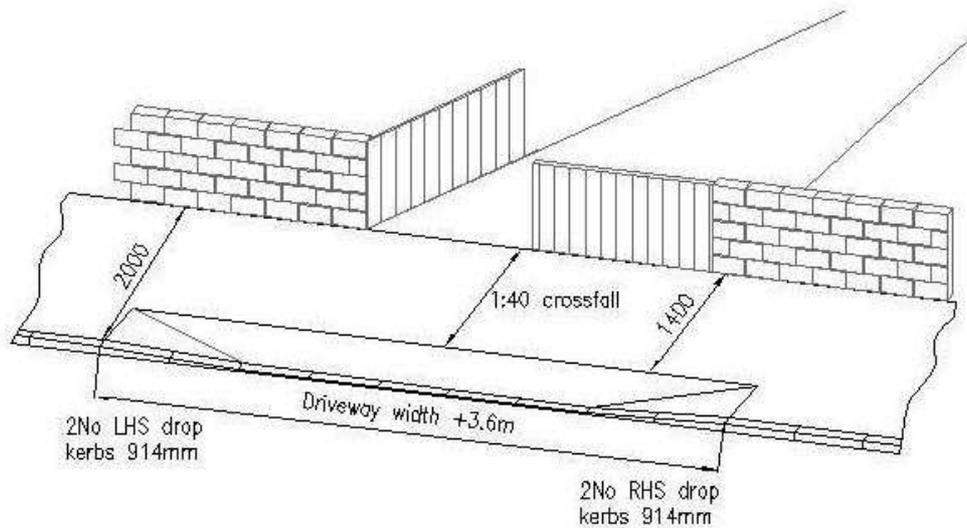
Notes:

1. Verge a minimum of 500mm wide to rear of bituminous surface.
2. Service bay may be constructed on left or right hand side.
3. Gates should be set 8m from road edge and open inwards.
4. Surfacing to extend a minimum of 6m from carriageway edge.
5. Sub-base depth may vary depending on road/ground conditions and type.



PLAN AND SECTION OF SERVICE BAY TO A RURAL DEVELOPMENT OF UP TO 4 HOUSES

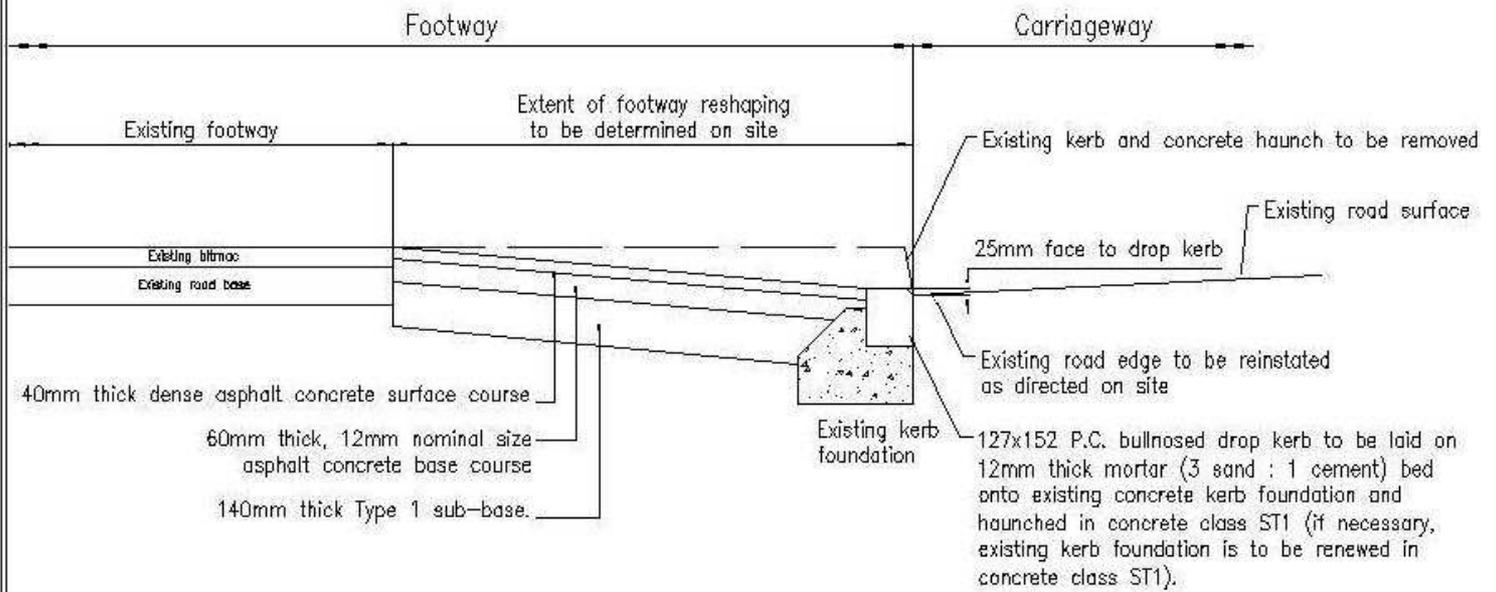
DRAWING No SDB 2



Notes:

1. Normal kerb upstand 100mm
2. Max drop kerb upstand 25mm

Scale 1:100



SPECIFICATION

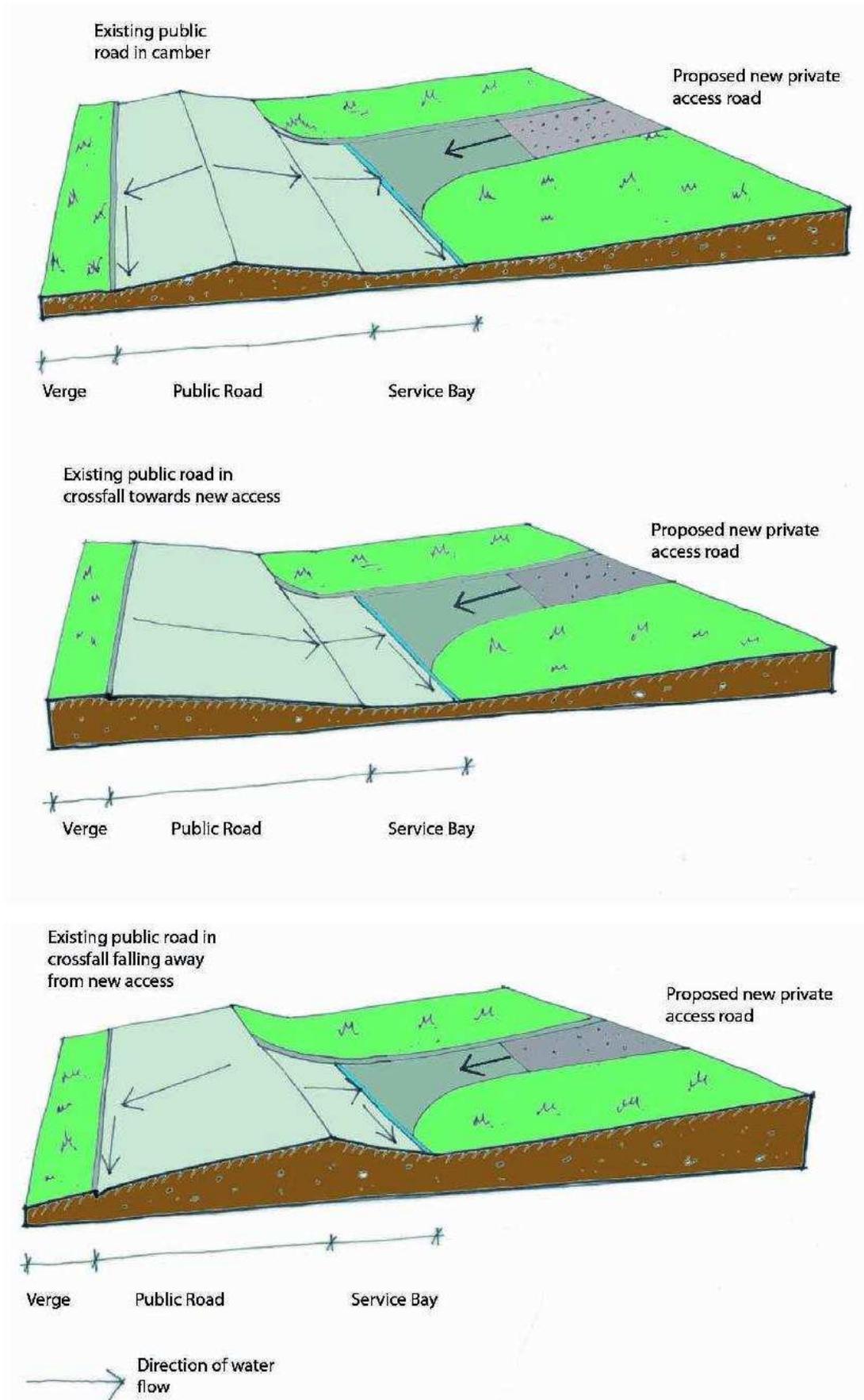
Scale 1:20

TYPICAL FOOTWAY CROSSING DETAILS

DRAWING No SDB 3

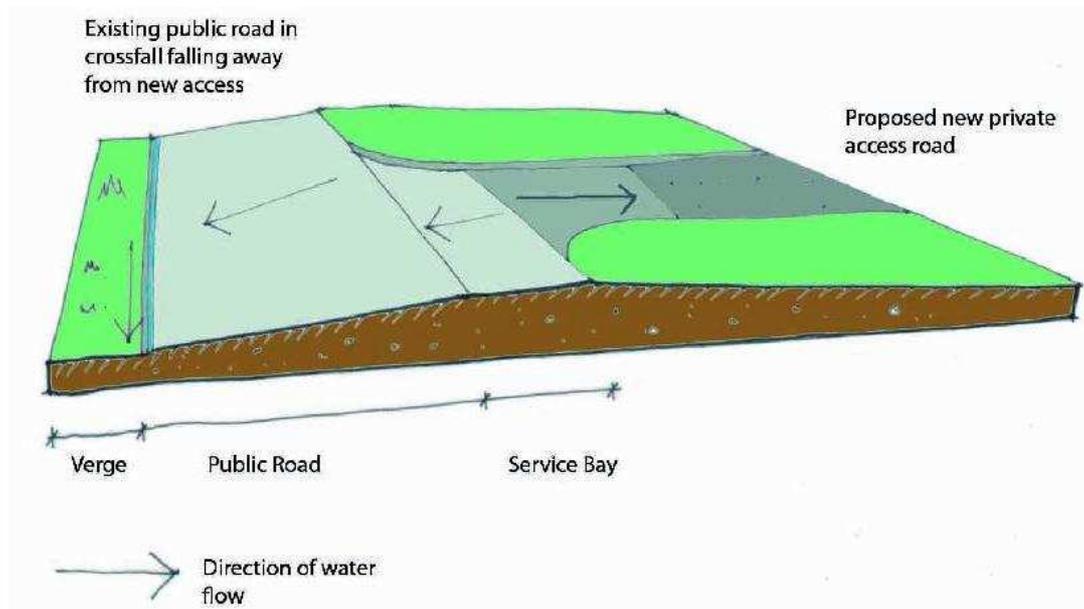
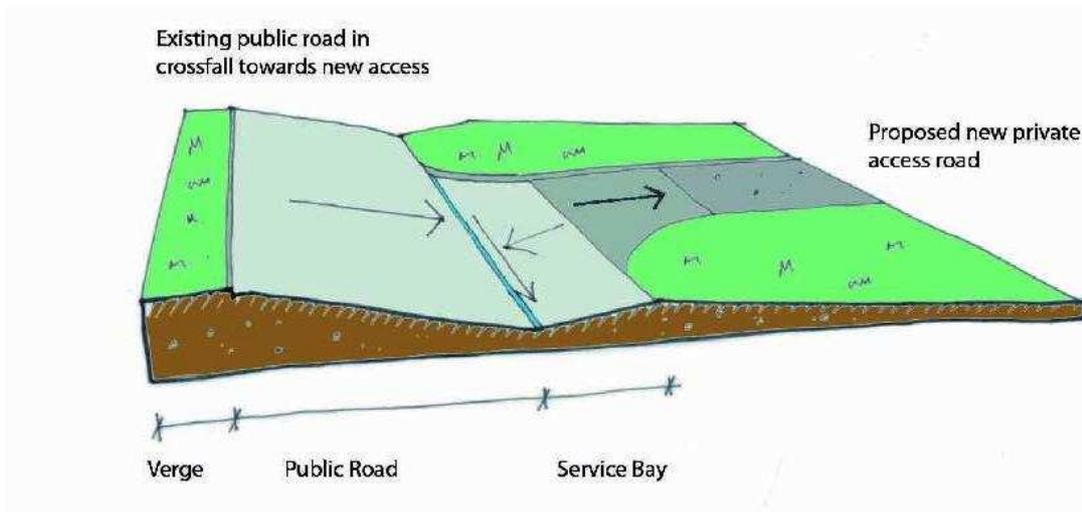
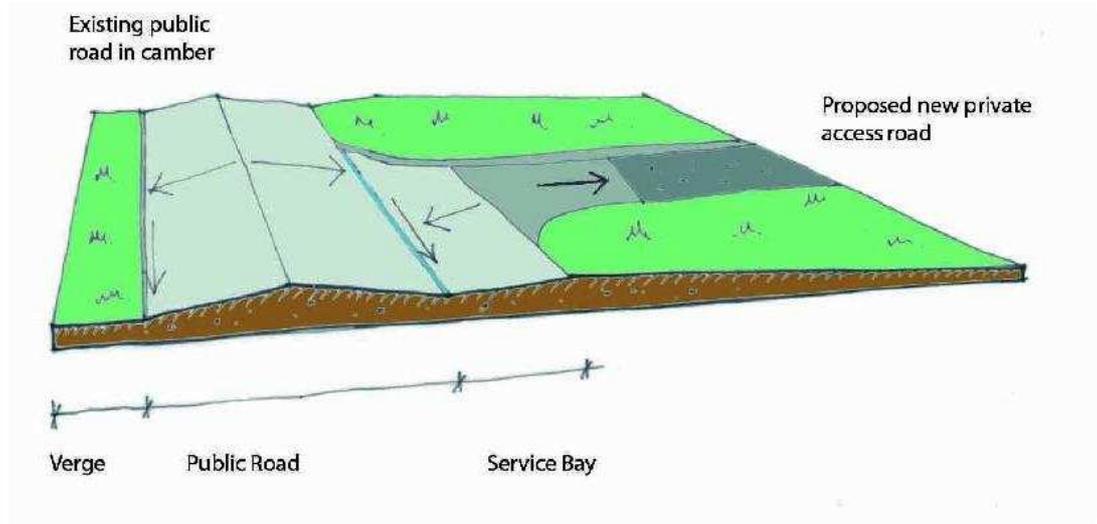
ACCESS ARRANGEMENT DRAINAGE PROFILES

ACCESS RISING FROM A PUBLIC ROAD



ACCESS ARRANGEMENT DRAINAGE PROFILES

ACCESS FALLING FROM A PUBLIC ROAD



APPENDIX No 2
PÀIPEAR-TAICE ÀIR 2

PRIVATE ACCESS CHECKLIST
LIOSTA-SGRÙDAIDH AIRSON INNTRIGEADH
PRÌOBHAIDEA

Private Access Checklist

Liosta-sgrùdaidh airson Inntrigeadh Prìobhaideach

(To be included with your Planning Application)

This form should be completed in connection with applications for single houses and small housing developments (generally four houses or less). Guidance on access to this type of development from the public road is given in document **Access to Single Houses and Small Housing Developments** to which reference should be made. The clause references below relate to this document.

DETAILS OF THE SITE		
	Circle as appropriate	For guidance refer to clause
Is there a previous Application for this location?	YES / NO	
If YES above give the application reference number if known:	<input style="width: 100%;" type="text"/>	
National grid ref at the access point to the public road:	<input style="width: 100%;" type="text"/>	
Do you propose a new private access from the public road?	YES / NO	
Do you propose enlarging an existing private access?	YES / NO	
Do you propose sharing an existing private access?	YES / NO	4.1.4
How many properties will the access serve?	<input style="width: 100%;" type="text"/>	
Will the new access cross a footway?	YES / NO	5.1
Do you propose constructing a new Service Bay?	YES / NO	4.1
Do you propose utilising an existing Service Bay?	YES / NO	

DETAILS OF THE PUBLIC ROAD		
		For guidance refer to clause
Is the Private Access located on a class A or B road?	YES / NO	4.1.4
Road/Street name or number	<input style="width: 100%;" type="text"/>	
What Speed Limit applies to this Road/ Street?	<input style="width: 100%;" type="text"/>	4.3.1
What is the assessed traffic speed near the access in MPH	<input style="width: 100%;" type="text"/>	4.3.2
How close to the nearest Road Junction is your proposed access in metres?	<input style="width: 100%;" type="text"/>	4.1.4

VISIBILITY

What visibility distances will you be able to achieve from your access junction in metres?		For guidance refer to clause 4.2, 4.3, 5.2, 5.3
Looking left	<input type="text"/>	
Looking right	<input type="text"/>	
(NOTE - Dimensioned visibility splays must be shown on the site plan)		
Is the area of land within the Visibility Splays in the Developer's Ownership or Control and/or within the road boundary?	YES / NO	4.4, 5.3
Do walls or fences require to be set back to provide the required visibility?	YES / NO	4.2

DRAINAGE

Have you inspected the existing Roadside Drainage?	YES / NO	For guidance refer to clause 4.7
Do you propose installing a new Pipe Culvert?	YES / NO	4.7
If Yes above, what diameter of pipe culvert is proposed?	<input type="text"/>	4.7

FLOOD RISK

Has the SEPA Flooding Database been examined? See www.sepa.org.uk/flooding	YES / NO	For guidance refer to clause 4.9.2
Is the site considered to have any Flood Risk?	YES / NO	4.9.1
Has a flood risk assessment been undertaken by a competent person?	YES / NO	4.9.2
Are any major ditches or watercourses near or above the site?	YES / NO	

PARKING

Can parking for a minimum of two vehicles per dwelling be provided within the site in addition to any garage parking?	YES / NO	For guidance refer to clause 4.5.1, 5.5
Can turning space within the site be provided in addition to parking?	YES / NO	4.5.1, 4.5.2, 5.5

ROAD OPENING PERMIT

For guidance
refer to clause

Do you intend to carry out any of the following?

6.1

Excavate in the Verge, Footway, or Road	YES / NO
Make a water or sewerage pipe connection to the mains	YES / NO
Excavate for a service connection to Gas, Telecom, or Electricity	YES / NO
Lay a septic tank outfall across the public road	YES / NO

If the answer is YES to any of the above, then a Road Opening Permit is required by you or your contractor obtainable from the Highland Council (your local TECS office)

DETAILS OF THE APPLICANT or AGENT

Applicant or Agents name and Address:

Postcode:

E-Mail Address:

I HEREBY CERTIFY THAT, TO THE BEST OF MY KNOWLEDGE AND BELIEF, THE INFORMATION GIVEN ON THIS FORM IS CORRECT AND COMPLETE.

Signature

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Date

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FOR OFFICIAL USE ONLY

COMMENTS:

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