

Agenda Item	6.6
Report No	PLS/37/25

HIGHLAND COUNCIL

Committee: South Planning Applications Committee
Date: 18 June 2025
Report Title: 24/05253/FUL: West Fraser Europe Ltd
Land 870M NE Of Norbord Europe Ltd, Dalcross
Report By: Area Planning Manager

Purpose/Executive Summary

Description: Construction of a rail sidings yard including new rail sidings and connection to the main line, associated gantry crane (and/or reach-stackers), areas of hard standing, access road, vehicle parking, fencing, drainage, landscaping and associated infrastructure works and facilities

Ward: 17 – Culloden and Ardersier

Development category: Major

Reason referred to Committee: Major Development

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

Recommendation

Members are asked to agree the recommendation to **GRANT** the application as set out in section 11 of the report.

1. PROPOSED DEVELOPMENT

- 1.1 Planning permission is sought for the construction of a rail sidings yard including new rail sidings and a connection to the main line, associated gantry crane (and/or reach-stackers), areas of hard standing, access road, vehicle parking, fencing, drainage, landscaping and associated infrastructure works and facilities at the West Fraser Europe site, Dalcross, Inverness. The application is accompanied by an Environmental Impact Assessment Report (EIAR) and associated Non-Technical Summary (NTS).
- 1.2 The proposal comprises of two lengths of rail sidings, approximately 560m in length, located immediately south of the Aberdeen – Inverness rail line; a connection to the main line to the west of the sidings; and the installation of a gantry crane (and/or reach stacker) to carry out the loading and unloading of containers. The gantry crane is anticipated to be between 24m and 30m in height, Above Ordnance Datum. In addition, an area of hardstanding in the form of a reinforced concrete slab, will be constructed to facilitate the loading and unloading of containers.
- 1.3 Ancillary works will consist of three lanes for the storage of containers; a parking area for articulated lorries and trailers; and an entry and exit road. An office building, in the form of a portable building, and additional car parking is also proposed.
- 1.4 An area of woodland measuring 2.36 hectares will require to be felled, with replacement planting on a like-for like basis within the application site boundary to compensate for this. In addition, proposals now also include measures to manage an 8ha area of conifer woodland adjacent to the site to create conditions conducive to the development of an ancient woodland.
- 1.5 Access to the site is via the existing access from the A96 Trunk Road ((A96(T))). Two wetland ponds will be created and a SUDS scheme installed.
- 1.6 The applicant's supporting Planning Statement indicates that the development will greatly contribute to the creation of a sustainable operational future for the West Fraser site. It is anticipated that approximately 20,000 HGV movements will be removed from the road network in favour of transportation by rail and that CO2 emission reductions are predicted to be in the region of 9,000 tonnes.
- 1.7 Pre Application Consultation (PAC): The applicant carried out the statutory pre-application consultation by hosting two public events. The first event was held on 08 June 2024 at Croy Village Hall and the second event took place on 27 June 2024 at the same location. In addition, a free publicly accessible project website was created to host information relating to the proposals. According to the statutory pre-application consultation report submitted with the planning application, the website went live to coincide with the PAC advert appearing in the press and remained live to allow for sufficient time before and after the events for the public to make comments. Further information is contained within the pre-application report that accompanies this application.
- 1.8 Supporting Information: The following information relevant to the determination of this application has been submitted:

- Environmental Impact Assessment Report and Non-Technical Summary;
- Pre-application Consultation Report; and
- Planning Statement

1.9 Variations: Biodiversity enhancement and environmental mitigation measures increased.

2. SITE DESCRIPTION

2.1 The site is an area of open land in the countryside lying immediately to the northeast of the existing West Fraser Europe's factory complex located approximately 2.5 kilometres southwest of Inverness Airport and to the north of the A96(T). It immediately adjoins the Aberdeen – Inverness railway line. A large proportion of the site is woodland. The factory comprises an Oriented Strand Board (OSB) mill.

2.2 The development site extends to the northeast of the existing site and southeast of the neighbouring Aberdeen – Inverness railway line and the planning application red line boundary extends to an area of approximately 11.7 hectares, with the longest length alongside the existing railway measuring approximately 783m. The western portion of the site is characterised by marshy shrubland, with two existing wetland ponds, while the eastern portion of the site is covered by woodland. To the southwest is the OSB mill, timber storage yards, associated buildings and infrastructure. To the south of the site lies farmland, with the A96 located approximately 350m from the southern boundary of the site.

3. PLANNING HISTORY

3.1	02 May 2024 (Date of Submission)	24/01598/PAN: Construction of a rail sidings yard, connection to the main line, associated gantry crane and associated infrastructure works and facilities	Decision: N/A
3.2	15 August 2024	24/00532/PREMAJ: The Development comprises two rail sidings to the south of the existing Aberdeen-Inverness rail line, a connection to the main line to the West of the sidings, the use of a gantry crane to carry out the loading and unloading of containers and associated works.	Response Issued
3.3	29 August 2024	24/01594/SCOP: Installation of two rail sidings and associated infrastructure, to connect West Fraser's OSB mill at Morayhill with the Aberdeen to Inverness railway line	Scoping Decision Issued

4. PUBLIC PARTICIPATION

4.1 Advertised: EIA Development
 Date Advertised: 07 March 2025
 Representation deadline: 06 April 2025

Timeous representations: One from one household

Late representations: N/A

4.2 Material considerations raised are summarised as follows:

- a) Additional landscaping should be provided;
- b) Additional information on tree removal should be submitted;
- c) Clarification required on types of lighting;
- d) Impact from noise;
- e) Air quality impacts; and
- f) Construction and operational site access.

4.3 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet www.wam.highland.gov.uk/wam.

5. CONSULTATIONS

5.1 **Ardersier and Petty Community Council:** No response received.

5.2 **Historic Environment Team (Archaeology):** No objection. The site lies within an area of archaeological potential. A condition requiring a Written Scheme of Investigation (WSI) should be imposed on any permission granted.

Should the archaeological works reveal the need for post excavation analysis the development hereby approved shall not be occupied or brought into use unless a Post-Excavation Research Design (PERD) for the analysis, publication and dissemination of results and archive deposition has been submitted to and approved in writing by the Planning Authority. The PERD shall be carried out in complete accordance with the approved details.

The archaeological planning condition requires that the development area is the subject of an evaluation in the first instance in order to establish the archaeological content and potential. Dependent on the results of this work, further study may be required in advance of, and during, construction works to record any identified remains. The evaluation will be backed up by desk-based research to produce a report setting out the results and any required mitigation strategy. The applicant will need to engage the services of a professional archaeological contractor.

5.3 **Historic Environment Team (Conservation):** No Objection. There appear to be no listed buildings within the development area to be affected by this proposal, or within the wider context whose setting could be impacted.

5.4 **Development Plans Team:** Subject to the views of relevant consultees on flood risk, forestry and biodiversity enhancement matters the proposal is likely to be in overall conformity with the approved development plan. Developer contributions may be required.

5.5 **Ecology Team:** No objection, subject to the imposition of recommended conditions. Biodiversity Enhancement: This site complies with the requirements of NPF4 Policy 3. The additional information submitted by the applicant on 25 April 2025 details the further information required to assess whether the proposal would meet the

requirements of NPF4 Policy 3. The Ecology team is satisfied that the additional enhancement measures are sufficient to meet Policy 3. This includes 8ha of woodland which will be managed and restructured to diversify, and an area of additional pastureland which will be managed as neutral grassland.

- 5.6 **Environmental Health (from Scoping Opinion):** The existing operations at the West Fraser site currently fall within the scope of a PPC (Prevention of Pollution Control) permit issued by SEPA which covers noise emissions from the site. Clarification is required as to whether noise arising from the proposed development will also be covered by the permit. In such circumstances, Environmental Health would not normally suggest Planning conditions which might differ from or contradict anything within the permit.

Members will be advised at Committee of any additional conditions that may require to be imposed, should planning permission be granted for the proposal.

- 5.7 **Forestry Team:** No objection. Since the Forestry team's previous consultation response, the applicant has provided a letter from Ramboll to address the issues raised by the Ecology and Forestry teams, along with a revised Figure 9-8 Landscape Mitigation Plan (Rev P4).

It is now proposed to carry out 2.36ha of on-site compensatory planting through increasing the area of Scots pine planting and reducing the grassland areas, while the broadleaf planting areas remain the same. This is considered acceptable to the Forestry team.

- 5.8 **Flood Risk Management Team (FRM):** No objection subject to conditions.

The site is within an area that is predicted to be at medium risk of river and 'surface water and small watercourse' flooding on SEPA's future flood maps. Under NPF4 only specific types of infrastructure can be located in areas at risk of flooding. Following discussion with Planning, FRM is satisfied that the proposed sidings are considered essential infrastructure that have to be at this location for operational reasons. The applicant must demonstrate that the flood risk is understood and that there will be no increase in flood risk to others as a result of the development.

FRM has reviewed the Technical Note (Ramboll 23/05/2025). This states that a detailed Flood Risk Assessment (FRA) can be carried out prior to works on site and if compensatory storage is required there is space to accommodate this on site. FRM is content with this approach and have consequently withdrawn their earlier objection to the application.

The applicant shall provide an updated FRA that considers the current (baseline) flood risk in and around the site. This shall include updated hydrology and hydraulic modelling. The FRA should also include the proposed situation with the new development, including the extended culverts, in place. This will need to demonstrate that the flood risk to the site is understood, there will be no loss of floodplain and that there will be no increase in flood risk to others. FRM has requested that this is secured through a suitably worded condition.

The FRA includes details of the surface water drainage strategy. Runoff will be directed to SUDS in the form of two new wetland ponds within the site. FRM is content with this approach and will require a condition that the final surface water drainage design is submitted for review.

- 5.9 **Transport Planning Team:** No objection. The submission suggests that this development will be accessed by private routes directly from the A96 Trunk Road. Given this, Transport Planning recommend that feedback is sought from Transport Scotland on the suitability of the vehicular access arrangements proposed and the predicted vehicular impacts onto their A96(T).

From a local roads perspective, Transport Planning needs to be satisfied that routing of construction and ongoing operational traffic generated by this development does not inflict unacceptable impacts onto local roads in that area. This can be checked through the consideration of suitable Construction and Operational Traffic Management Plans, which are recommended to be conditioned on any permission issued. The Construction Traffic Management Plan would need to be resolved prior to works commencing, with the Operational Traffic Management Plan needing to be resolved prior to each phase of the proposed development coming into operation.

Regarding the new car parking proposed within the site, Transport Planning note and welcome that provision is being made for disabled drivers and EV charging equipment. Subject to the above recommended conditions, Transport Planning support this development that should contribute towards reduced vehicle movements on the public roads across the Highlands by creating new rail freight opportunities associated with this large commercial facility.

- 5.10 **Civil Aviation Authority:** No response received
- 5.11 **HIAL:** No objection. HIAL's preliminary assessment shows that, at the given position and height, this development would not infringe the safeguarding criteria and operation of Inverness Airport.
- 5.12 **NATS:** No objection. The proposed development has been examined from a technical safeguarding aspect and does not conflict with NATS safeguarding criteria. Accordingly, NATS has no safeguarding objection to the proposal.
- 5.13 **NatureScot:** This proposal could be progressed with appropriate mitigation. However, because it could affect internationally important natural heritage interests, NatureScot object to this proposal unless it is made subject to conditions so that the works are done strictly in accordance with the mitigation detailed in NatureScot's consultation response.

Inner Moray Firth SPA

NatureScot's advice is that this proposal is likely to have a significant effect on the osprey qualifying interest of the SPA. No likely significant effect is predicted for the other qualifying interests. Consequently, Highland Council, as competent authority, is required to carry out an appropriate assessment in view of the site's conservation objectives for its qualifying interest. This has subsequently been carried out and is attached as Appendix 2 of this report.

Moray Firth SPA

NatureScot's advice is that it is unlikely that the proposal will have a significant effect on the qualifying interests either directly or indirectly. An appropriate assessment is therefore not required.

Moray Firth SAC

NatureScot's advice is that it is unlikely that the proposal will have a significant effect on the qualifying interests either directly or indirectly. An appropriate assessment is therefore not required.

Longman and Castle Stuart Bays SSSI

The proposal is located close to the Longman and Castle Stuart Bays SSSI notified for nationally important saltmarsh, mudflats, eelgrass beds, and non-breeding waterbirds. There are natural heritage interests of national importance on the site, but these will not be affected by the proposal.

Protected Species

NatureScot fulfil their advisory role on protected species through the provision of standing advice and do not expect to be consulted other than in exceptional circumstances not covered by the relevant standing advice.

Species protection plans should be produced for each protected species present, or potentially present, on the development site and incorporate the full range of protection measures identified in the EIA Report alongside any additional mitigation and/ or compensatory measures identified as necessary as a result of the pre-construction surveys. If changes in the use of the development site by protected species are identified, an updated assessment of the impacts of the proposal on them must be completed and appropriate mitigation measures identified (if required). If the implementation of the identified mitigation measures is not sufficient to avoid offences under protected species legislation, a licence will be required from NatureScot before works can proceed.

- 5.14 **Network Rail:** No objection. Network Rail is fully supportive of this development. However, due to its close proximity to the operational railway, Network Rail has asked that various matters relating to the detailed design and construction of the development are added as informatives to any permission granted. These have been included as informatives at the end of this report.
- 5.15 **SEPA:** No objection, subject to a condition requiring a Flood Risk Assessment to be submitted for the approval of the Council in consultation with SEPA.

Whilst SEPA maintain their requirement for additional modelling to be undertaken which suitably demonstrates pre and post development flood extents at the site and to show that there will be no reduction in floodplain capacity, SEPA is satisfied that the applicant has determined they will be able to demonstrate this prior to the development works commencing. The technical note indicates that they believe that there is adequate capacity within the red line boundary of the site to accommodate any compensatory storage should this be demonstrated to be required by the results of an updated flood risk assessment.

While it would be preferable to have this information upfront, SEPA recognise that in this site-specific scenario, the principle of the development accords with NPF4 Policy 22 and the risks can be assumed by the applicant and likely contained within the site boundary. However, SEPA highlight that by undertaking the updates to the flood modelling by condition, the applicant assumes a level of commercial risk should the results of the modelling demonstrate requirements for compensatory storage or impacts to the site layout.

- 5.16 **Transport Scotland:** No objection. Recommend that conditions are imposed on any permission granted requiring (i) a Construction Traffic Management Plan (CTMP) to be submitted to, and approved by, the Planning Authority in consultation with Transport Scotland, before commencement of development, and thereafter all construction traffic conforming to the requirements of the CTMP; and (ii) prior to the movement of any components and / or construction materials, any additional signing or temporary traffic control measures deemed necessary due to the size or length of any abnormal loads being transported must be undertaken by a recognised QA traffic management consultant, to be approved by Transport Scotland.

6. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

6.1 National Planning Framework 4 (2023) (NPF4)

Policy 1 - Tackling the Climate and Nature Crises
Policy 2 - Climate Mitigation and Adaptation
Policy 3 - Biodiversity
Policy 4 - Natural Places
Policy 5 - Soils
Policy 6 - Forestry, Woodland and Trees
Policy 7 - Historic Assets and Places
Policy 13 - Sustainable Transport
Policy 14 - Design Quality and Place
Policy 18 - Infrastructure First
Policy 20 - Blue and Green Infrastructure
Policy 22 - Flood Risk and Water Management
Policy 23 - Health and Safety
Policy 25 - Community Wealth Building
Policy 26 - Business and Industry
Policy 29 - Rural Development

6.2 Highland Wide Local Development Plan 2012 (HwLDP)

28 - Sustainable Design
29 - Design Quality and Place-making
31 - Developer Contributions
36 - Development in the Wider Countryside
41 - Business and Industrial Land
51 - Trees and Development
52 - Principle of Development in Woodland
56 - Travel
57 - Natural, Built and Cultural Heritage
58 - Protected Species
59 - Other important Species
60 - Other Importance Habitats
63 - Water Environment
64 - Flood Risk
66 - Surface Water Drainage
74 - Green Networks

6.3 **Inner Moray Firth Local Development Plan 2 (2024) (IMFLDP2)**

- 1 – Low Carbon Development
- 2 – Nature Protection, Preservation and Enhancement
- 7 – Industrial Land
- 8 – Placemaking
- 9 – Delivering Development and Infrastructure
- 14 - Transport

6.4 **Highland Council Supplementary Planning Policy Guidance**

- Biodiversity Planning Guidance (May 2024)
- Construction Environmental Management Process for Large Scale Projects (August 2010)
- Developer Contributions (March 2018)
- Flood Risk and Drainage Impact Assessment (Jan 2013)
- Green Networks (Jan 2013)
- Highland Historic Environment Strategy (Jan 2013)
- Highland's Statutorily Protected Species (March 2013)
- Managing Waste in New Developments (March 2013)
- Physical Constraints (March 2013)
- Standards for Archaeological Work (March 2012)
- Sustainable Design Guide (Jan 2013)
- Trees, Woodlands and Development (Jan 2013)

7. **OTHER MATERIAL POLICY CONSIDERATIONS**

7.1 None.

8. **PLANNING APPRAISAL**

8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

Determining Issues

8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

Planning Considerations

8.3 The key considerations in this case are:

- a) compliance with the development plan and other planning policy
- b) Siting and design
- c) Environmental impacts (topics scoped into EIAR)
- d) Impact on infrastructure and services and proposed mitigation (developer contributions)

e) any other material considerations

Development plan/other planning policy

- 8.4 The site lies outwith any land allocation or Settlement Development Area in the IMFLDP2, meaning the site lies within the countryside in the context of the HwLDP and is rural development in terms of NPF4.
- 8.5 NPF4 Policies 1-3 apply to all development proposals nationwide. When considering proposals, significant weight will be given to the global climate and nature crises. Development proposals will be sited and designed to minimise lifecycle greenhouse gas emissions as far as possible. Development proposals will contribute to the enhancement of biodiversity, including where relevant, restoring degraded habitats and building and strengthening nature networks and the connections between them. Proposals should also integrate nature-based solutions, where possible.
- 8.6 The key development plan policies which determine the acceptability or otherwise of the principle of development are NPF4 Policy 26 (Business and Industry) and Policy 29 (Rural Development); HwLDP Policy 36 Development in the Wider Countryside and Policy 41 Business and Industrial Land and IMFLDP2 Policy 7 Industrial Land.
- 8.7 NPF4 Policy 26 Part (d) i. requires it to be demonstrated that there are no allocated suitable alternative sites for business or industrial development. This is not considered necessary for this proposal as there is a clear locational and operational need for the chosen location. In terms of the nature and scale of the activity (Part (d) ii), given its location adjacent to the existing large scale manufacturing facility and the rail line, it is considered largely compatible with the surrounding area. In terms of the nature of the activity (Part (d) ii.) and impact on residential amenity (Part (e) i.) there is potential for impacts, particularly given there are a number of residential properties nearby. There is also potential for natural environment impacts due to woodland loss and flood mitigation.
- 8.8 The principle of development is consistent with NPF4 Policy 29 Rural Development Part (a) v). as it is considered the proposal meets the NPF4 glossary definition of essential infrastructure.
- 8.9 The principle of the development is generally consistent with HwLDP Policy 36 Development in the Wider Countryside, whereby the development is acceptable in terms of siting, design and is sympathetic with existing patterns of development in the area. HwLDP Policy 41 and IMFLDP2 Policy 7 give support for the principle of this type of proposal outwith allocated sites subject to detailed considerations, including amenity and environmental impacts.
- 8.10 Subject to the development being considered acceptable in relation to its siting and design, and having no significant environmental impacts, the proposal would comply with the Development Plan.

Siting and design

- 8.11 The main development site is located immediately northeast of the existing factory and adjacent to the Aberdeen – Inverness railway line. Two railway sidings will be constructed parallel with the main line with two gantry crane runways, with a length of

approximately 355 metres, on either side. Between the railway sidings and the second runway will be a 3.5-metre-wide road and 2 No. three-metre-wide container storage lanes. The elevation plans submitted with the proposal indicates that the storage containers will be stacked a maximum of four high.

- 8.12 As mentioned in paragraph 1.2, the gantry crane is anticipated to be between 24m and 30m in height. To put this in context with the existing factory, the industrial mill includes a main building approximately 51 metres in height. The description of the development in this application and in the supporting information mentions that the development will include a gantry crane and/or reach stackers, (the latter understood to be vehicles). In the event that a gantry crane is to be installed at the site, it is recommended that final design details are secured by a planning condition, prior to installation so that the exact design and specification is known beforehand.
- 8.13 A new 14-metre-wide access road will be constructed northwest of the factory servicing the site with a proposed car parking area leading to a lorry parking area capable of accommodating approximately 35 HGVs.
- 8.14 Beyond the hardstanding areas, on land to the southeast, two wetland ponds will be formed.
- 8.15 The extent of engineering works required to deliver the development will result in substantial earthworks taking place as areas of the site will need to be built up to meet required levels. The railway sidings and related infrastructure would be raised significantly, with the highest level of fill reaching 3.3 metres above existing ground levels. The supporting Planning Statement estimates cut and fill earthworks of approximately 65,000m³. Appendix 2.1 (EIA Scoping Report) of the EIAR advises that “materials to be used within the scheme will comprise site won materials as far as is practicable. Additional materials, including imported fill materials, will be sourced from recycled or secondary sources where possible and the use of primary aggregate will be minimised where possible within technical specifications.” (Para. 5.12.3.5). Furthermore, the EIA Scoping Report states that: “The cut and fill balance for the scheme is not yet known and the volumes of materials that may be considered unsuitable for use (chemically or geotechnically) are unknown. Given the peat nature of the geology beneath the site in the west, it is anticipated that some soils will not be suitable for reuse on site.” (Para. 5.12.3.14).
- 8.16 The supporting Planning Statement indicates that an office building, likely in the form of a portable building, is to be located adjacent to the car parking area. In the submitted plans, this is referred to as a ‘Gatehouse Accommodation’ building, measuring approximately 10 metres x 3 metres. The rectangular design differs slightly from the rectilinear footprint shown on the site layout plans. Final design details can be secured by condition.
- 8.17 A lighting scheme is also proposed as part of the development. The elevation plan and other supporting information shows a series of lighting columns throughout the site. The final design has not been established, and this can be covered by condition. It is noted that Chapter 10 of the EIAR suggests that in the event that a reach stacker is utilised, lighting columns will be required at typically 20 metres in height and, along with other lighting elements, will be included in the detailed lighting design scheme.

- 8.18 Given the clear operational requirements influencing the siting and design of the proposal, the details provided in the application are considered acceptable, subject to appropriate conditions as recommended at the end of this report to approve final design elements relating to the gatehouse building, lighting scheme, and gantry crane.

Environmental Impacts (topics scoped into EIAR)

8.19 Climate Change

The NTS advises that during the construction of the development, (use of raw materials, transport, and construction process itself) there would be a minor adverse and not significant effect on climate and identified receptors. As greenhouse gas emissions (GHG) would be released to the atmosphere, the global atmosphere is the receptor and is of high sensitivity.

- 8.20 Operational GHG emissions would be generated mainly from the use of the proposed development and its maintenance. However, the completed development would result in a long term permanent direct beneficial and significant effect on climate and identified receptors, primarily due to the reduction in road transportation.

8.21 Ecology and Biodiversity

The NTS advises that several sensitive features would be adversely affected by the development, including irreplaceable habitats such as long-established coniferous plantation and upland birch woodland listed in the Ancient Woodland Inventory, and Purple Moorgrass and rush pastures wetland habitat. Habitat loss due to construction works, felling of woodland and clearing of vegetation and groundworks, was identified.

- 8.22 Proposed enhancement measures, including habitat management techniques, are proposed to compensate for any damage or loss occurring due to construction works. Badgers, birds' nests and bats are statutorily protected, and a number of mitigation measures are proposed to avoid or minimise their disturbance.

- 8.23 The NTS reports that overall, construction of the proposed development would not be significant on ecology and identified receptors and consequently would not give rise to significant effects on ecology. In terms of the completed development, it is noted that the lighting scheme would result in an adverse effect on ecology and identified receptors, but this would not give rise to significant effects.

- 8.24 Subject to the detailed mitigation measures proposed in the EIAR report and other mitigation proposed following consultations with the Council's Ecology team and NatureScot, it is considered that the development would not have an unacceptable impact on ecology and biodiversity, and that biodiversity net gain can be adequately achieved. This will be controlled through conditions, as recommended at the end of this report, in the event that the Committee agrees to grant planning permission for the proposal.

8.25 Forestry

As highlighted earlier in this report, the proposals will result in a loss of an area of woodland. Replacement planting has been proposed to compensate for this, with additional planting and mitigation proposed since receipt of the application. The NTS

advises that whilst the impact on woodland would be significant, the proposed compensatory planting would mitigate this, and the overall effect considered minor/moderate adverse and therefore not significant.

8.26 Subject to appropriate conditions as recommended at the end of this report, the impact on woodland is considered to be acceptable.

8.27 **Landscape and Visual**

Chapter 9 of the EIAR considers potential impacts and the associated effects of development on the local landscape. Mature trees partially surround and the mill on the north, east and southwest sides, creating a natural barrier that separates the site from nearby farmland and dispersed housing.

8.28 The Site is within the Landscape Character Type (LCT) identified by NatureScot as "Coastland Farmlands – Moray and Nairn," as noted in Figure 7.1, which is mostly flat, open farmland. The surrounding areas include rolling farmland with woodlands, urban spaces, farmed valleys near Inverness, and a less developed coastal area along the inner firths.

8.29 The EIAR notes that during the construction stage, there are likely to be temporary, direct adverse effects upon the landscape of the site, and immediate context and site features themselves.

8.30 The NTS concludes that during the construction phase, the partial clearance of the existing site and construction of the proposed development will result in temporary landscape and visual effects, but these would not give rise to significant effects on landscape and visual amenity.

8.31 The EIAR assessed two scenarios relating to the completion of the development. Scenario 1 looks at years 0-3 years post construction. This scenario assumes the proposed development is operational, with newly planted compensatory vegetation including trees and shrubs, still immature and offering limited screening. Scenario 2 assesses landscape and visual impact 15 years post construction. Here, it is anticipated that the compensatory vegetation would have reached a substantial growth level, potentially providing effective screening from the proposed development.

8.32 Overall, the EIAR considered that the completed proposed development would result in long-term minor adverse landscape and visual effects; and as such would not give rise to significant effects for landscape and visual amenity.

8.33 **Lighting**

Chapter 10 of the EIAR details the likely effects of the proposal in relation to lighting. The NTS confirms that the proposed lighting design (as referenced in paragraph 8.16 of this report) which has not been prepared in advance of submission of this application, will address any required discharge of planning conditions.

8.34 The long-term lighting with embedded mitigation would need to specify sufficient detail to avoid the component of light most disturbing to bats; have appropriate lighting controls using motion sensors or similar where safe and practical to minimise light

impact; and be appropriately tilted to emit no upward light and not cause glare to sensitive receptors.

- 8.35 The NTS confirms that lighting for the gantry crane would be part of the crane equipment specification, and that the manufacturer would be required to submit, as a minimum, details of luminaire types, colour temperature, spectral distribution, photometric files, tilt, controls, and heights so the lighting impact from the crane can be fully assessed at the detailed design stage. The NTS asserts that it is likely that the downward lighting on top of the crane would have no impact on residents south of the site and mature treelines/ habitats. However, lighting of the staircases would likely increase visibility of the site, potentially causing glare to residents and increased lux levels to mature treelines/ habitats, unless additional mitigation measures are carried out. This will involve the use of shields/cowls to reduce light spill and direct it only to where it is needed.
- 8.36 As part of the embedded mitigation, the loading and unloading of containers to and from freight trains will be between the hours of 7am and 8pm, Monday to Saturday. As such, impact on residential receptors would be temporally limited and short-term in nature.
- 8.37 The main conclusion of the EIAR is that it is reasonably assumed that the proposed lighting and controls for the new development would ensure the magnitude of impact to residents and mature treelines/habitats would be negligible/not significant, but only with the appropriate additional mitigation measures applied. There would be a negligible/not significant impact on road users and train user receptors.
- 8.38 **Noise and Vibration**
Chapter 11 of the EIAR presents assessments of the potential noise impacts and likely effects associated with the construction stage and completed development stages of the proposed development (considered in phases). The assessments have been undertaken with reference to relevant national and local policies and guidance and in accordance with the methodology set out within the EIA Scoping Report. In accordance with the EIA Scoping Report, effects due to vibration during the construction and operational stages have been scoped out due to the nature of the development and the distance between the proposed development and the nearest receptors.
- 8.39 The EIAR advises that a Construction Environmental Management Plan (CEMP) will be submitted to the Council for approval prior to works commencing. With embedded mitigation measures, noise from one construction activity (embankment works occurring during the night-time) is expected to result in significant effects (temporary, short-term, direct, 'Major Adverse') at the Station House receptor group. However, no significant effects are expected at this receptor due to construction activity noise during the daytime. At all other receptors, noise from construction activity is not expected to result in any significant effects.
- 8.40 Post construction, the effects of operational noise have been assessed accounting for embedded mitigation comprising:
- Restrictions on the hours of operations for the proposed development and on the number of reach stackers being operated;

- Sound emission limits for the proposed reach-stacker (that are likely to require sensitive selection of this equipment and provision of acoustic cladding and exhaust attenuation); and
- Operation of an electric gantry crane with broadband movement alarms (rather than a diesel version with tonal movement alarms).

8.41 Additional mitigation has been considered to reduce the potential for significant effects due to operational noise. This additional mitigation comprises restrictions on idling of locomotives within the proposed sidings and a requirement to carry out noise monitoring and further assessment of real-world operational noise emissions. Where this assessment identifies likely significant effects, further noise mitigation measures shall be implemented (such as acoustic screening, if appropriate).

8.42 **Transport and Access**

Chapter 12 of the EIAR reports on the likely significant effects on receptors along the transport routes anticipated to be used by vehicles associated with the construction stage and completed development stage of the proposed development.

8.43 The EIAR predicts that construction traffic will result in a temporary increase in traffic flows on the road network surrounding the proposed development. The maximum traffic increase associated with construction of the proposed development is predicted to occur during the delivery of fill material and will equate to 70 construction vehicles per day for approximately eight weeks, and this will comprise the peak construction period. Staff transport cars and LGVs would account for a maximum of 40 vehicle movements (20 inbound trips and 20 outbound trips) per day during the peak period of construction. The peak daily construction period is therefore estimated to result in a total of 180 vehicle movements, comprising 40 Car / LGVs movements and 140 HGV movements.

8.44 The traffic generation during the completed development phase scenarios would see a reduction in operational trips on the road network compared to the existing situation and therefore no receptors are considered likely to experience significant effects in accordance with the EIA Regulations. The NTS notes that a Construction Traffic Management Plan will be provided to manage construction traffic. This can be controlled by condition.

8.45 **Water Environment**

Chapter 13 of the EIAR deals with the likely significant water environment effects of the proposed development. The assessment of construction and operation stages has been undertaken taking into account the relevant national and local guidance and regulations.

8.46 A desk study was conducted to establish the existing baseline water environment conditions using various data sets. The Site includes ponds, drainage channels, tributary watercourses, and the Rough Burn, a nearby watercourse which discharges into the Moray Firth. The area is underlain by the Hillhead Sandstone Formation with superficial deposits such as Peat, Alluvium, and the Alturlie Gravels.

8.47 During construction, no effects would be significant if unmitigated. The proposed development's impact on groundwater resources and flood risk during construction is

expected to be 'Minor Adverse' but managed with mitigation measures, causing no significant medium-term or long-term adverse effects. All other construction effects were considered to be 'Negligible Adverse' and 'Not Significant'.

- 8.48 The NTS advises that during operation, the mitigation measures embedded within the design of the proposed development would manage any adverse impacts. This would be achieved through the two newly constructed wetland ponds which would attenuate and treat surface water to ensure there is no impact on water quality or quantity discharge to Rough Burn. With the mitigation in place, the effect is considered to be 'Minor Beneficial' and 'Not Significant'.
- 8.49 Chapter 13 of the EIAR determines that although there could be disruptions during construction and operation activities, long-term impacts are anticipated to be non-significant due to planned mitigation strategies and site design, which includes substantial drainage, culvert design, and the establishment of wetland ponds.
- 8.50 It is noted (as referenced in the Consultation Section of this report) that SEPA and the Council's Flood Risk Management Team remain to be satisfied that flood risk has been fully understood and mitigated. Consequently, a condition is recommended to be imposed requiring a Flood Risk Assessment to be submitted and approved by the Planning Authority, in consultation with SEPA and the Flood Risk Management Team.

Impact on infrastructure and services and proposed mitigation (developer contributions)

- 8.51 Impact on infrastructure and proposed mitigation measures have been examined and reported in the EIAR. Overall, it is considered that the proposed development will not have a significant adverse impact, and indeed in terms of impact on climate change and the roads infrastructure, the development will see a key reduction in carbon emissions and HGVs on the road. Consequently, developer contributions will not be required for this development.

Other material considerations

- 8.52 The applicant has provided a written response to the various matters raised in the third-party representation that was received. This includes information on additional screen planting proposed; lighting; noise; and air quality management.

8.53 Community wealth building

Following consultation, the Highland Council's Community Wealth Building Strategy 2024-2027 was agreed by the Council in September 2024 subject to minor amendments. The strategy provides a framework that sets out how the Council will utilise different activities to maximise the impact of investment in local areas and support more local ownership of assets and wealth.

- 8.54 The supporting Planning Statement explains that the proposal contributes to community wealth building in the local area through the 9,000-tonne reduction in CO2 emissions and the reduction of road traffic, contributing to a more sustainable environment. It further explains that the extension and investment into the current West Fraser operations will likely result in an increase in staff, and long-term employment security which will further benefit the local community. It is considered

that these measures will satisfy the requirements of NPF4 Policy 25 relating to community wealth building.

Non-material considerations

8.55 None.

Matters to be secured by Legal Agreement / Upfront Payment

8.56 None.

9. CONCLUSION

9.1 The proposal is welcomed given its potential for significant environmental benefits over time, particularly in terms of reduced carbon emissions and other benefits of a reduction in the number of HGV journeys on the public road network. The principle of the proposal is consistent with NPF4, HwLDP and IMFLDP2. In particular, it is considered that the proposal meets the definition of essential infrastructure in NPF4 and that there are clear public benefits arising from the proposal that allow the principle of the location within a flood risk area, and the loss of woodland, to be acceptable, subject to appropriate mitigation for both being provided.

9.2 The Council is satisfied that environmental effects of this development can be addressed by way of mitigation. The Council has incorporated the requirement for a schedule of mitigation within the conditions of this permission. Monitoring of operational compliance will also be secured through the imposition of the recommended conditions attached to this report.

9.3 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

10. IMPLICATIONS

10.1 Resource: Not applicable

10.2 Legal: Not applicable

10.3 Community (Equality, Poverty and Rural): Not applicable

10.4 Climate Change/Carbon Clever: Not applicable

10.5 Risk: Not applicable

10.6 Gaelic: Not applicable

11. RECOMMENDATION

Action required before decision issued N

Notification to Scottish Ministers N

Conclusion of Section 75 Obligation	N
Revocation of previous permission	N

Recommended to **GRANT** the application subject to the following conditions and reasons:

1. The development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

Reason: In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended).

2. No works in connection with the development hereby approved shall commence unless an archaeological Written Scheme of Investigation (WSI) has been submitted to and approved in writing by the Planning Authority and a programme of archaeological works has been carried out in accordance with the approved WSI. The WSI shall include details of how the recording and recovery of archaeological resources found within the application site shall be undertaken, and how any updates, if required, to the written scheme of investigation will be provided throughout the implementation of the programme of archaeological works. Should the archaeological works reveal the need for post excavation analysis, the development hereby approved shall not be occupied or brought into use unless a Post-Excavation Research Design (PERD) for the analysis, publication and dissemination of results and archive deposition has been submitted to and approved in writing by the Planning Authority. The PERD shall be carried out in complete accordance with the approved details.

Reason: In order to protect the archaeological and historic interest of the site.

3. No development works shall commence until a suitable Flood Risk Assessment (FRA) that considers the current (baseline) flood risk in and around the site, has been submitted to, and approved in writing by, the Planning Authority in consultation with SEPA and the Council's Flood Risk Management Team.

The FRA shall include updated hydrology and hydraulic modelling which clearly demonstrates pre and post development flood extents during a 0.5% AEP event (including a 42% climate change allowance in line with current SEPA guidance) accompanied by a range of suitably justified blockage scenarios.

The FRA shall also include the proposed situation with the new development, including the extended culverts, in place. This will need to demonstrate that the flood risk to the site is understood and that there will be no loss of floodplain and that there will be no increase in flood risk to others.

The FRA shall demonstrate no increased flood risk being passed downstream and no reduction in floodplain capacity at the site.

In the event that floodplain capacity is reduced, a scheme for compensatory flood storage shall be submitted for the consideration and approval of the Planning Authority in consultation with SEPA and the Council's Flood Risk Management Team.

Should compensatory flood storage be required, this shall be provided for within the development site boundary prior to the completion and first operation of the development.

Reason: To ensure that adequate measures are put in place to manage flood risk, in the interests of environmental protection.

4. No development shall commence until a pre-construction osprey survey report, to a distance of 750m from the site boundary, has been undertaken with the survey to be submitted to, and approved in writing by, the Planning Authority in consultation with NatureScot.

The report shall include a Species Protection Plan for osprey and shall include, as appropriate:

- (i) Strategies to reduce noise and visual disturbance;
- (ii) Appropriate mitigation such as adjustments to construction program to avoid potential disturbing activities between late March to September when osprey are reliant on their nest sites; and
- (iii) Dedicated ornithological monitoring to provide guidance and compliance records.

The development shall thereafter be undertaken in accordance with the agreed mitigation measures contained within report.

Reason: To ensure that the site and its environs are surveyed and the development does not have an adverse impact on ospreys.

5. No development shall commence until a Habitat Management Plan (HMP) [taking account of the Outline/Draft Habitat Management Plan], has been submitted to, and approved in writing by the Planning Authority.

The HMP shall set out proposed habitat management of the site during the period of construction, operation, and decommissioning, restoration and aftercare, and shall provide for the maintenance, monitoring and reporting of site-specific details or particular species, habitats or wetlands on site.

The HMP shall provide provision and details for regular monitoring and review to be undertaken against the HMP objectives and reasonable measures for securing amendments or additions to the HMP in the event that the HMP objectives are not being met.

Until otherwise approved in advance in writing by the Planning Authority, the approved HMP (as amended from time to time with written approval of the Planning Authority) shall be implemented in full in line with the timescales set out in the approved plan.

Reason: In the interests of good land management and the protection of habitats.

6. No development shall commence until a Construction and Environmental Management Plan (CEMP) containing site specific details of all on-site construction works, post-construction reinstatement, drainage and mitigation, together with details of their timetabling, has been submitted to, and approved in writing by, the Planning Authority.

The CEMP shall include (but is not limited to):

- (a) Risk assessment of potentially damaging construction-type activities on the environment;
- (b) Mitigation to protect the ecological resources on site, including biodiversity protection zones, location and timing of works;
- (c) Species and Habitat Protection Plans, (including badgers, red squirrel, amphibians and breeding birds);
- (d) Site specific details for management and operation of any concrete batching plant (including disposal of pH rich waste water and substances);
- (e) A Pollution Prevention and Incident Plan incorporating a Pollution Prevention Plan, Pollution Incident Plan and a Pollution Control Monitoring Plan, which shall provide measures to protect watercourses groundwater, management of natural surface hydrological flows (flushes, springs, etc.).
- (f) A surface water and groundwater management and treatment plan, including details of the separation of clean and dirty water drains, and location of settlement lagoons for silt laden water;
- (g) Details of measures to be taken to prevent loose or deleterious material being deposited on the local road network including wheel cleaning and lorry sheeting facilities, and measures to clean the site entrances and the adjacent local road network;
- (h) Details of temporary site illumination, including measures to ensure light spill/pollution is minimised and avoids habitats within the site and does not extend beyond the immediate working area, and not beyond the site boundary;
- (i) Details of construction noise monitoring; physical mitigation measures; and community liaison procedures, including the Contractor's response to real-time alerts; and
- (j) Details of construction hours of working.

The approved CEMP shall be implemented throughout the construction, post-construction site reinstatement and operational phases in full unless otherwise approved in advance in writing by the Planning Authority.

Reason: To ensure that all construction operations are carried out in a manner that minimises their impact on the environment, and that the mitigation measures which accompanied the application, or as otherwise agreed, are fully implemented.

7. No development shall commence until a pre-construction survey for protected species, is undertaken no more than three months prior to works commencing and a report of the survey submitted to, and approved in writing by, the

Planning Authority. The survey shall cover both the application site and an appropriate buffer of at least 100 metres from the boundary of the application site and the report of survey shall include mitigation measures where any impact, or potential impact, on protected species or their habitat has been identified.

Development and work shall progress in accordance with any mitigation measures contained within the approved report of survey and the timescales contain therein.

Reason: To ensure that all construction operations are carried out in a manner that minimises their impact on protected species, and that the mitigation measures which accompanied the application, or as otherwise agreed, are fully implemented.

8. No development shall commence until a Construction Traffic Management Plan (CTMP) has been submitted to, and approved in writing by, the Planning Authority, in consultation with Transport Scotland and the Roads Authority. Thereafter, all construction traffic associated with the development shall conform to the requirements of the agreed CTMP.

Reason: To mitigate the adverse impact of construction traffic on the safe and efficient operation of the trunk road and local road network.

9. Prior to any site excavation or groundworks, a Tree Protection Plan in accordance with BS 5837:2012 (Trees in Relation to Design, Demolition and Construction) shall be submitted to and subsequently approved in writing by the Planning Authority.

Reason: To ensure the protection of retained trees during construction and thereafter.

10. No development, site excavation or groundwork shall commence until all retained trees have been protected against construction damage using protective barriers located as per the approved Tree Protection Plan and in accordance with BS 5837:2012 Trees in Relation to Design, Demolition and Construction, or any superseding guidance prevailing at that time. These barriers shall remain in place throughout the construction period and shall not be moved or removed during the construction period without the prior written approval of the Planning Authority.

Reason: In order to ensure the protection of retained trees, which are important amenity assets, both during construction and thereafter.

11. No development, including tree felling, shall commence until a detailed Compensatory Planting Plan (including future maintenance) based on the Landscape Mitigation Plan has been submitted and approved in writing by the Planning Authority, following consultation with Scottish Forestry and any other relevant stakeholders.

The area of planting shall be no less than 2.36 hectares in size, consisting of 1.61ha of Scots pine woodland and 0.75ha of broadleaf/ mixed woodland, and located within the Highlands.

The area identified for compensatory planting may also need to be considered under The Forestry (Environmental Impact Assessment) (Scotland) Regulations 2017, where this exceeds the current thresholds.

The Compensatory Planting Plan shall follow the same process as required for preparing a woodland creation proposal, as set out in the Scottish Forestry publication: Woodland Creation Application Guidance.

The Compensatory Planting Plan shall be prepared by and then implemented under the supervision of a suitably qualified forestry consultant, approved by the Planning Authority. The appointed forestry consultant shall provide a detailed schedule of supervision, with compliance monitoring reports to be issued at agreed stages.

The approved Compensatory Planting Plan shall be implemented in full, prior to first commissioning of the development. The compensatory planting shall be maintained thereafter in accordance with the approved scheme, until established to the full satisfaction of the Planning Authority and then shall remain as woodland in perpetuity.

To comply with the Felling Permission exemptions, woodland removal shall not begin until the applicant can demonstrate that construction work is imminent. In the event that development fails to commence within 3 years of the initial felling, then the land use shall revert back to woodland and the area shall be replanted within 12 months, to a specification approved by the Planning Authority.

Where compensatory planting takes place on land located outside the planning application boundary and/or is not under the ownership of the applicant, agreement in writing from the landowner shall be provided to the Planning Authority.

The applicant shall provide the Planning Authority with a GIS shapefile clearly identifying the approved area(s) of woodland removal and the associated area(s) of compensatory planting.

Reason: To protect Scotland's woodland resource, in accordance with the Scottish Governments policy on the Control of Woodland Removal.

12. GIS Shapefiles shall be supplied to the Planning Authority of all the proposed enhancement areas prior to the commencement of works.

Reason: To allow the compensation and enhancement areas to be mapped to ensure no developments occur on these sites for a minimum of 30 years.

13. No works relating to the installation of the gantry crane shall commence until full details and specifications for the gantry crane have been submitted to, and approved in writing by, the Planning Authority. This shall include full details and specifications of the design and height of the gantry crane, along with any

required external lighting. Thereafter, development and work shall progress in accordance with the approved details.

Reason: In order to enable the Planning Authority to consider these matters in detail prior to the commencement of development; in the interests of amenity.

14. No development relating to the siting/installation and/or construction of any buildings shall be carried out until full details of the design, layout and external finishes have been submitted to, and approved in writing by, the Planning Authority. Thereafter, development and work shall progress in accordance with the approved details.

Reason: In order to enable the Planning Authority to consider these matters in detail prior to the commencement of development; in the interests of amenity.

15. No development relating to the installation of any operational external lighting shall be carried out until full details of the finalised external lighting scheme have been submitted to, and approved in writing by, the Planning Authority. Such information shall include full details of the location, type, angle of direction and wattage of each light which shall be so positioned and angled to prevent any direct illumination, glare or light spillage outwith the site boundary. Thereafter only the approved details shall be implemented.

Reason: In order to safeguard the amenity of neighbouring properties and occupants.

16. Prior to installation, details of the final surface water drainage design for the development shall be submitted to, and approved in writing by, the Planning Authority. Thereafter, development and works shall progress in accordance with the approved surface water drainage design and be completed before first operation of the development.

Reason: To enable consideration of this matter in the interests of environmental protection.

17. Any stacked containers shall not exceed a maximum of four containers high.

Reason: In the interests of visual amenity.

REASON FOR DECISION

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

REASONED CONCLUSION

The Council is in agreement with the findings of the Environmental Impact Assessment Report that the proposed development is unlikely to give rise to any new or other significant adverse impact on the environment. The Council is satisfied that all environmental effects of this development can be addressed by way of mitigation. The Council has incorporated the requirement for a schedule of mitigation within the conditions of this permission. Monitoring of operational compliance has been secured through the planning conditions imposed on this permission.

INFORMATIVES

Initiation and Completion Notices

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

Network Rail

The following matters should be fully agreed with Network Rail prior to works commencing on site:

- All proposed rail connections, new sidings and any required changes to existing railway infrastructure;
- The design, location and maintenance of any Sustainable Urban Drainage Scheme;
- The design, location and maintenance of any proposed fencing;
- The design, colour, location and maintenance of any proposed lighting;
- Where trees/shrubs are to be planted adjacent to the railway boundary these should be positioned at a minimum distance from the boundary which is greater than their predicted mature height. Certain broad leaf deciduous species should not be planted adjacent to the railway boundary; and
- All construction works adjacent to the operational railway must be undertaken in a safe manner which does not disturb the operation of the existing railway.

Applicants must be aware of any embankments and supporting structures which are in close proximity to their development.

The developer should contact our Asset Protection Engineers regarding the above matters by [submitting an enquiry on the Network Rail website](#)

Further information regarding working on or near the railway can be found on the [Network Rail website](#).

Transport Scotland – Abnormal Loads

Prior to the movement of any components and/or construction materials, any additional signing or temporary traffic control measures deemed necessary due to the size or length of any abnormal loads being transported, shall be undertaken by a recognised QA traffic management consultant, to be approved by Transport Scotland. This is to ensure that the transportation of abnormal loads will not have any detrimental effect on the trunk road network.

Scottish Water

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

Local Roads Authority Consent

In addition to planning permission, you may require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2

Mud and Debris on Road

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy

for dealing with any material deposited on the public road network and maintain this until development is complete.

Construction Hours and Noise-Generating Activities

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact env.health@highland.gov.uk for more information.

Protected Species – Halting of Work

You are advised that work on site must stop immediately, and NatureScot must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from NatureScot: <https://www.nature.scot/professional-advice/protected-areas-and-species/protected-species>

Signature:	Bob Robertson
Designation:	Area Planning Manager - South
Author:	John Kelly
Background Papers:	Documents referred to in report and in case file.
Relevant Plans:	Plan 1 - Location Plan
	Plan 2 - Proposed Site Layout Plan
	Plan 3 - Proposed Elevation Plan
	Plan 4 - Floor Plan
	Plan 5 - Proposed General Arrangement

- Plan 6 - Elevations – Boundary Fence
- Plan 7 - Floor/Elevation Plan - Gatehouse
- Plan 8 - Elevations – Lighting Column
- Plan 9 - Proposed Earthworks Plan
- Plan 10 - Site Section Plan - Earthworks
- Plan 11 - Proposed Surfacing Plan
- Plan 12 - Section Plan - High Tensile Fence
- Plan 13 - Section Plan – Welded Mesh Fence
- Plan 14 - Landscaping Plan - Mitigation

The Highland Council

Appropriate Assessment

Inner Moray Firth Special Protection Area

Proposal: Construction of a rail sidings yard including new rail sidings and connection to the main line, associated gantry crane (and/or reach-stackers), areas of hard standing, access road, vehicle parking, fencing, drainage, landscaping and associated infrastructure works and facilities

THC Ref. 24/05253/FUL

CONSIDERATION OF PROPOSALS AFFECTING EUROPEAN SITES

The status of the Inner Moray Firth Special Protection Area (SPA) means that the requirements of the Conservation (Natural Habitats, & c.) Regulations 1994 as amended (the 'Habitats Regulations') or, for reserved matters the Conservation of Habitats and Species Regulations 2017 as amended apply.

The above means that where the conclusion reached by the Council on a development proposal unconnected with the nature conservation management of a Natura 2000 site is that it is likely to have a significant effect on those sites, it must undertake an Appropriate Assessment of the implications for the conservation interests for which the areas have been designated. The need for Appropriate Assessment extends to plans or projects out with the boundary of the site in order to determine their implications for the interest protected within the site.

This means that the Council, as competent authority, has a duty to:

- Determine whether the proposal is directly connected with or necessary to site management for conservation; and, if not,
- Determine whether the proposal is likely to have a significant effect on the site either individually or in combination with other plans or projects; and, if so, then
- Make an Appropriate Assessment of the implications (of the proposal) for the site in view of that site's conservation objectives.

The competent authority can only agree to the proposal after having ascertained that it will not have an adverse effect on the integrity of the sites. If this is not the case and there are not alternative solutions, the proposal can only be allowed to proceed if there are imperative reasons of overriding public interest, which in this case can include those of a social or economic nature.

Appraisal Summary

Inner Moray Firth Special Protection Area (SPA)

In its response to the Council, NatureScot advised that the proposal is likely to have a significant effect on the osprey qualifying interest of the SPA. No likely significant effect is predicted for the other qualifying interests of the SPA. However, if the proposal is carried out strictly in accordance with the mitigation, set out in NatureScot's consultation response dated 08 May 2025, NatureScot's conclusion is that the proposal will not adversely affect the integrity of the SPA.

While the responsibility to carry out the Appropriate Assessment rests with the Council, advice contained within Circular 6/1995 is that the assessment can be based on the information submitted from other agencies. In this case, the Appropriate Assessment is informed by information supplied by NatureScot.

APPROPRIATE ASSESSMENT

HIGHLAND COUNCIL APPRAISAL OF THE PROPOSAL IN RESPECT OF INNER MORAY FIRTH SPECIAL PROTECTION AREA (SPA)

Ospreys normally nest in large, mature trees (alive or dead) with flat tops (5–40 m from the ground), often selecting trees that are prominent within their surroundings. Extensive stands of Long Established of Plantation Origin 2b (LEPO 2b) woodland cover the site (and surrounds) and are to be impacted by the proposal. The woodland is characterised by native tree species such as Scots pine as the dominant species and frequent silver birch. Rowan is also frequent, and juniper is occasionally present. The woodland is structurally diverse with young and mature trees alternating (*Section 7.8.9, EIA Report Chapter 7 Ecology and Biodiversity*). Therefore, it is considered possible that osprey could potentially nest within disturbance distance, taken to be 350-750m with a medium/ high sensitivity to disturbance [Disturbance Distances in selected Scottish Bird Species – NatureScot Guidance | NatureScot](#)

The applicant has not undertaken appropriate survey to rule out, or inform, this possibility. *Section 7.8.37, EIA Report Chapter 7 Ecology and Biodiversity* states “no surveys for birds were undertaken as part of the field surveys”. Therefore, it is currently unknown whether osprey breed within disturbance distance and as such there remains the potential for significant effects with regards to disturbance impacts from construction and operation. Therefore, a Species Protection Plan should be prepared by the applicant which demonstrates how the potential risk of disturbance to osprey would be mitigated. This should include comprehensive surveys by an experienced surveyor to identify any nest sites prior

to any works commencing. Ospreys arrive from their wintering grounds in late March/ early April and remain until late August/ early September.

Although disturbance distances are convenient for quantification of bird responses to human disturbance, it should be recognised that bird heart rate may be increased by exposure to human disturbance before alert behaviour or flight initiation responses are evident. Increased heart rate and increased levels of stress hormones have physiological costs and so disturbance may have subtle impacts even on birds that are not clearly showing behavioural responses to disturbance. Therefore, the upper limit of the disturbance distance should be used unless it can be demonstrated that a lower distance is sufficient.

It is recommended that the applicant consults the Highland Raptor Study Group to confirm the status and location of nearby osprey nests at an appropriate time prior to the start of works. However, it should be acknowledged that this data may only constitute background information, may not be available, or may be incomplete, and the applicant will need to undertake their own surveys.

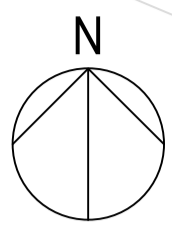
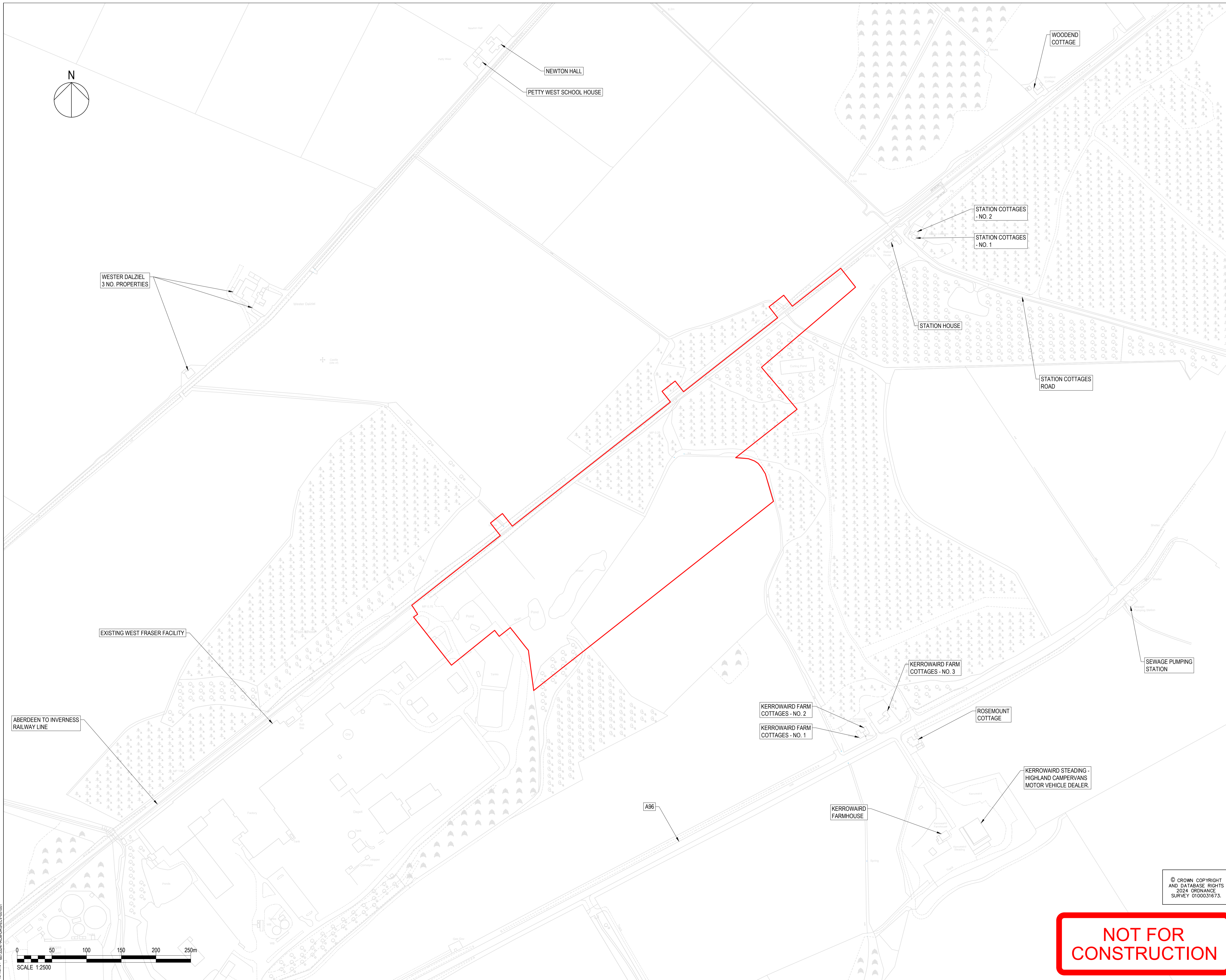
Furthermore, osprey are listed on Schedule 1 and therefore receive additional protection which makes it an offence to intentionally or recklessly to disturb any bird while it is building a nest, any bird while is in, on, or near a nest containing eggs or young, or the dependent young of any bird. Further information is available at: <https://www.nature.scot/professional-advice/protected-areas-and-species/protected-species/protected-species-z-guide/protected-species-birds>

Provided the proposal is carried out strictly in accordance with the following mitigation, the Council's conclusion is that the proposal will not adversely affect the integrity of the site:

- Pre-construction osprey survey, to a distance of 750m from the site boundary, to be undertaken prior to works commencing;
- A Species Protection Plan for osprey to be agreed with NatureScot, once above survey results are available, which should include, as appropriate:
- Strategies to reduce noise and visual disturbance;
- Appropriate mitigation such as adjustments to construction program to avoid potential disturbing activities between late March to September when osprey are reliant on their nest sites; and
- Dedicated ornithological monitoring to provide guidance and compliance records.

The Highland Council

04 June 2025



SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
2. DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
3. ALL DIMENSIONS IN MILLIMETRES. ALL CHANGINGS, LEVELS AND COORDINATES ARE IN METRES UNLESS DEFINED OTHERWISE.
4. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
5. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001002 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001003 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - EARTHWORKS CROSS SECTIONS
 - 60733247-ACM-DRG-ECV-001006 - PROPOSED SURFACING PLAN
 - 60733247-ACM-DRG-ECV-001007 - ELEVATIONS

KEY:

- SURVEY / OS MAPPING
- EXTENT OF PERMANENT WORKS

ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	05/07/24	P01
Revision Details	By	Check	Date	Suffix

Purpose of issue **FOR PLANNING**



Project Title **WEST FRASER**

Drawing Title **LOCATION PLAN**

Designed	Drawn	Checked	Approved	Date
LD	LD	NM	DH	July 24
AECOM Internal Project No.		Suitability	Project Manager	
60733247		-	David Martin	
Scale @ A1		Zone/ELRM/Stage	david.martin@aecom.com	
1:2500		ANI 3	0141 202 0500	

© CROWN COPYRIGHT AND DATABASE RIGHTS 2024. ORDNANCE SURVEY 0100031673.

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, MODIFIED, REPRODUCED OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING.

AECOM Infrastructure & Environment UK Limited
177 Bothwell Street
Glasgow
G2 7ER
Tel: +44 (0)141 202 0500
Fax:
www.aecom.com

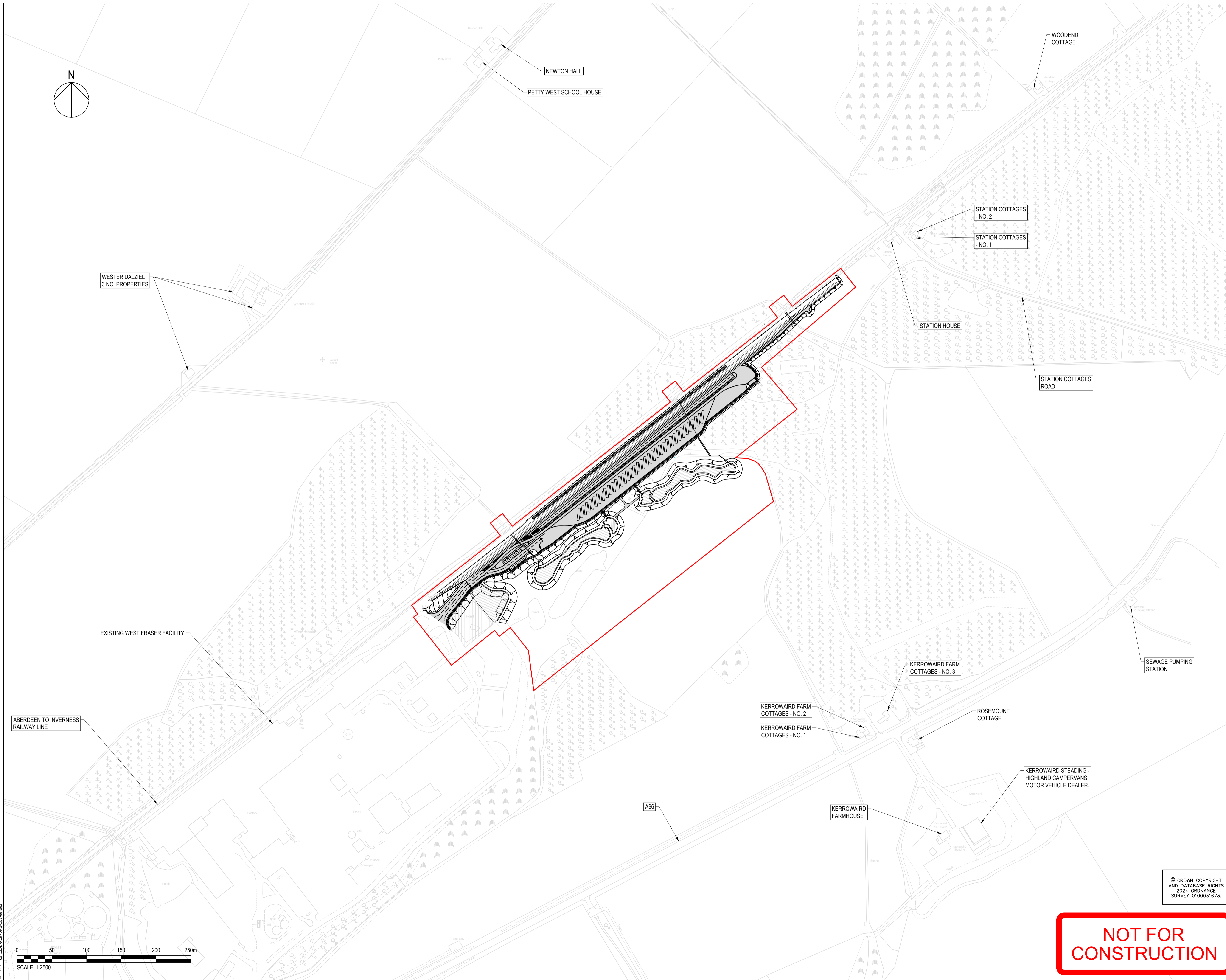


NOT FOR CONSTRUCTION

Drawing Number **60733247-ACM-DRG-ECV-001001** Rev **P02**

Plot Date: 6/15/2024 2:00 PM
 File Name: 60733247-ACM-DRG-ECV-001001





SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
2. DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
3. ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS DEFINED OTHERWISE.
4. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
5. PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
6. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001001 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001002 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - EARTHWORKS CROSS SECTIONS
 - 60733247-ACM-DRG-ECV-001006 - PROPOSED SURFACING PLAN
 - 60733247-ACM-DRG-ECV-001007 - ELEVATIONS

KEY:

- PROPOSED WORKS
- SURVEY / OS MAPPING
- EXTENT OF PERMANENT WORKS

REISSUED FOR PLANNING WITH POND ACCESS TRACK INCLUDED	LD	NM	04/12/24	P02
ISSUED FOR PLANNING	LD	NM	16/08/24	P01
Revision Details	By	Check	Date	Suffix

Purpose of Issue **FOR PLANNING**



Project Title **WEST FRASER**

Drawing Title **SITE PLAN**

Designed LD	Drawn LD	Checked NM	Approved DH	Date August 24
AECOM Internal Project No. 60733247		Subsidiary -	Project Manager David Martin	
Scale @ A1 1:2500		Zone/ELR/Mileage ANI 3	david.martin@aecom.com 0141 202 0500	

© CROWN COPYRIGHT AND DATABASE RIGHTS 2024. ORDNANCE SURVEY 0100031673.

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, MODIFIED, REPRODUCED OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING.

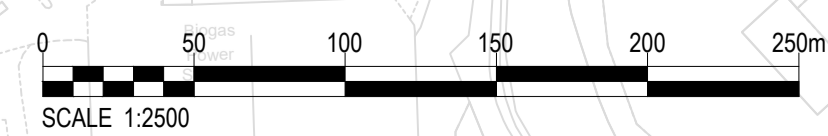
AECOM Infrastructure & Environment UK Limited
177 Bothwell Street
Glasgow
G2 7ER
Tel: +44 (0)141 202 0500
Fax:
www.aecom.com

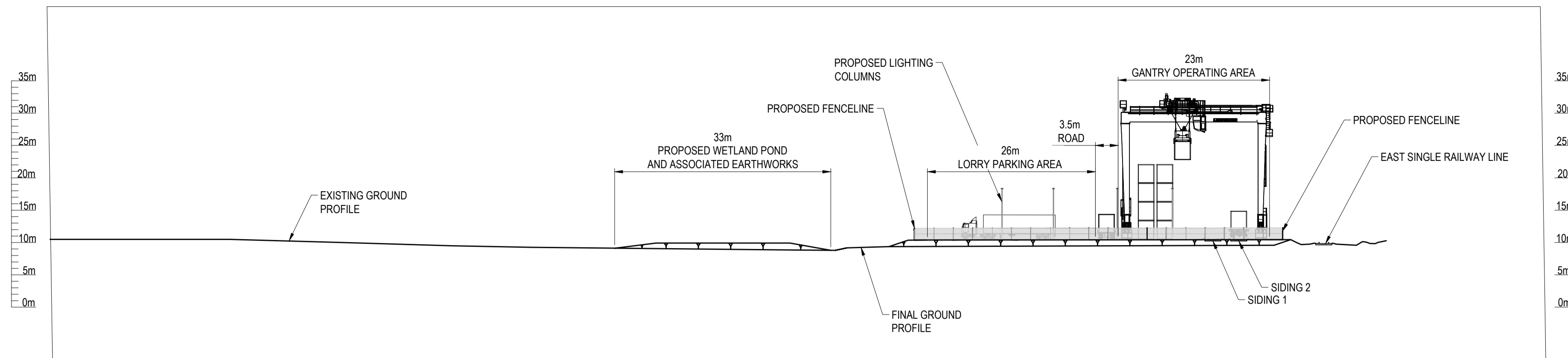


Drawing Number	Rev
60733247-ACM-DRG-ECV-001003	P02

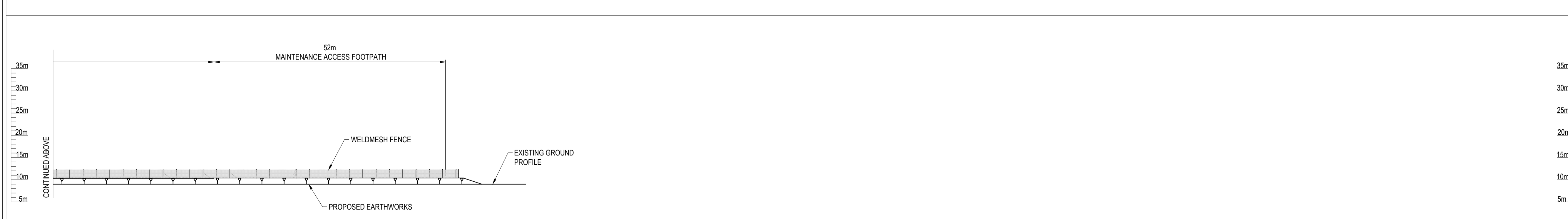
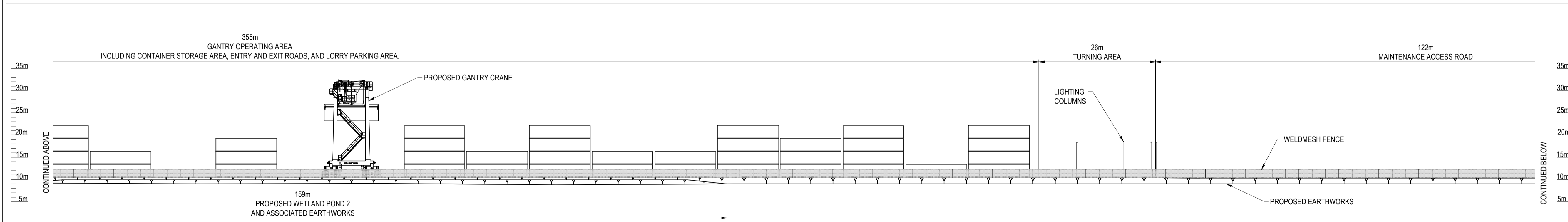
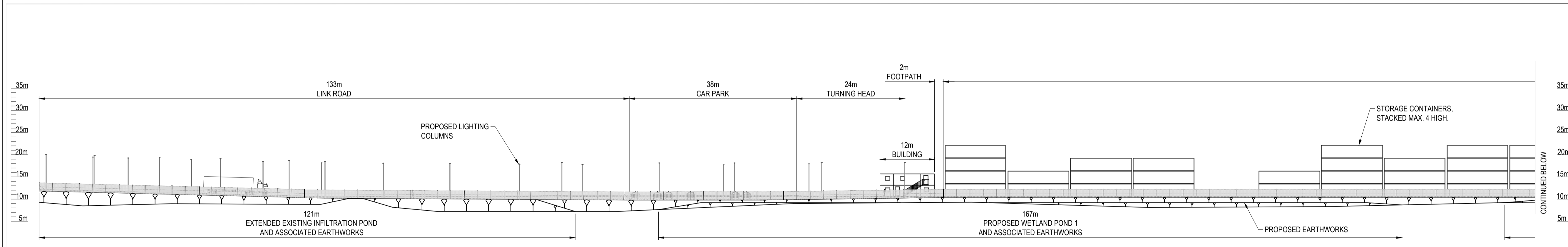
NOT FOR CONSTRUCTION

Plot Date: 13/08/2024 2:41 PM
File Name: 60733247-ACM-DRG-ECV-001003

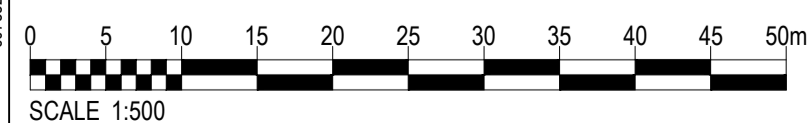




ELEVATION A
(001002)
FACING WEST TOWARDS FACTORY



ELEVATION B
(001002)
FACING NORTH TOWARDS TERMINAL



SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
2. DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
3. ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS DEFINED OTHERWISE.
4. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
5. PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
6. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001001 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001002 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001003 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - EARTHWORKS CROSS SECTIONS
 - 60733247-ACM-DRG-ECV-001006 - PROPOSED SURFACING PLAN
7. GANTRY CRANE DETAIL TAKEN FROM LIEBHERR TYPICAL DRAWING FOR RTG MODEL 664/WS DATED 22/05/2013 PROVIDED BY WEST FRASER ON 18/06/2024.

NOT FOR CONSTRUCTION

ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	31/07/24	P01
Revision Details	By	Check	Date	Suffix

Purpose of issue
FOR PLANNING



Project Title
WEST FRASER

Drawing Title
ELEVATIONS

Designed LD	Drawn LD	Checked NM	Approved DH	Date July 24
AECOM Internal Project No. 60733247		Subsidiary -	Project Manager David Martin	
Scale @ A1 1:500		Zone/ELR/Message ANI 3	david.martin@aecom.com 0141 202 0500	

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, MODIFIED, REPRODUCED OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING.

AECOM Infrastructure & Environment UK Limited
177 Bothwell Street
Glasgow
G2 7ER
Tel: +44 (0)141 202 0500
Fax:
www.aecom.com

Drawing Number 60733247-ACM-DRG-ECV-001007	Rev P02
---	------------

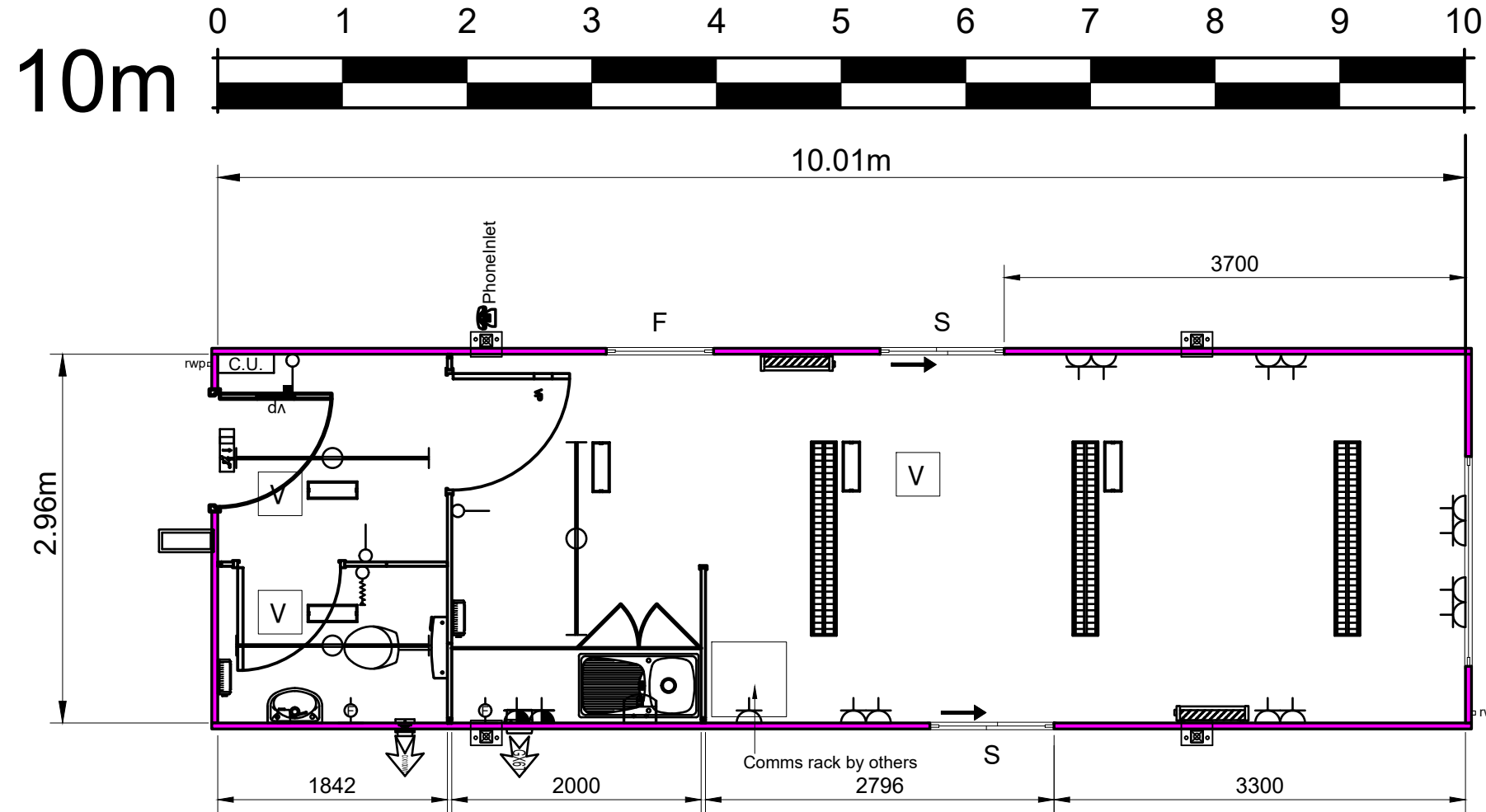
Plot Date: 6/07/2024 9:52 AM
 File Name: 60733247-ACM-DRG-ECV-001007

SL102 Solus Building

copyright Portakabin Limited 2017

This drawing is the property of Portakabin Limited.
It must not be copied or reproduced or divulged to anyone without written permission.

Do not scale off this drawing.
All discrepancies to be reported to Portakabin Limited.



Rev:	Date:	Description:	By:	Chk:
A	13.07.18	Scale bar added	AMV	JS
B	29.01.19	Client design changes made	AMV	JS

POWER

- C.U. Single phase consumer unit
- 2 gang switched socket
- Fused spur
- 2 gang switched socket (Mounted above worktop)

HEATING

- 2KW H/L fan Heater
- Frost Stat (for High Level Heater)
- 2KW convector heater

WATER HEATING

- 3KW oversink water heater
- 2KW undersink water heater

LIGHTING

- 1 x 58w, 1.5m fluorescent fitting c/w CPD51 diffuser
- 2 x 35w, 1.5m fluorescent fitting LG3/7

SWITCHING

- 1 gang, 1 way light switch
- 1 way pull switch

VENTILATION

- 100mm extract fan c/w time delay
- 150mm extract fan c/w over run timer

EMERGENCY LIGHTING

- 3hr combined N/M emergency light
- 3hr, N/M emergency light
- 3hr, N/M emergency light c/w legend
- Emergency light key test switch

MISCELLANEOUS

- Polyflor Vinyl Covering

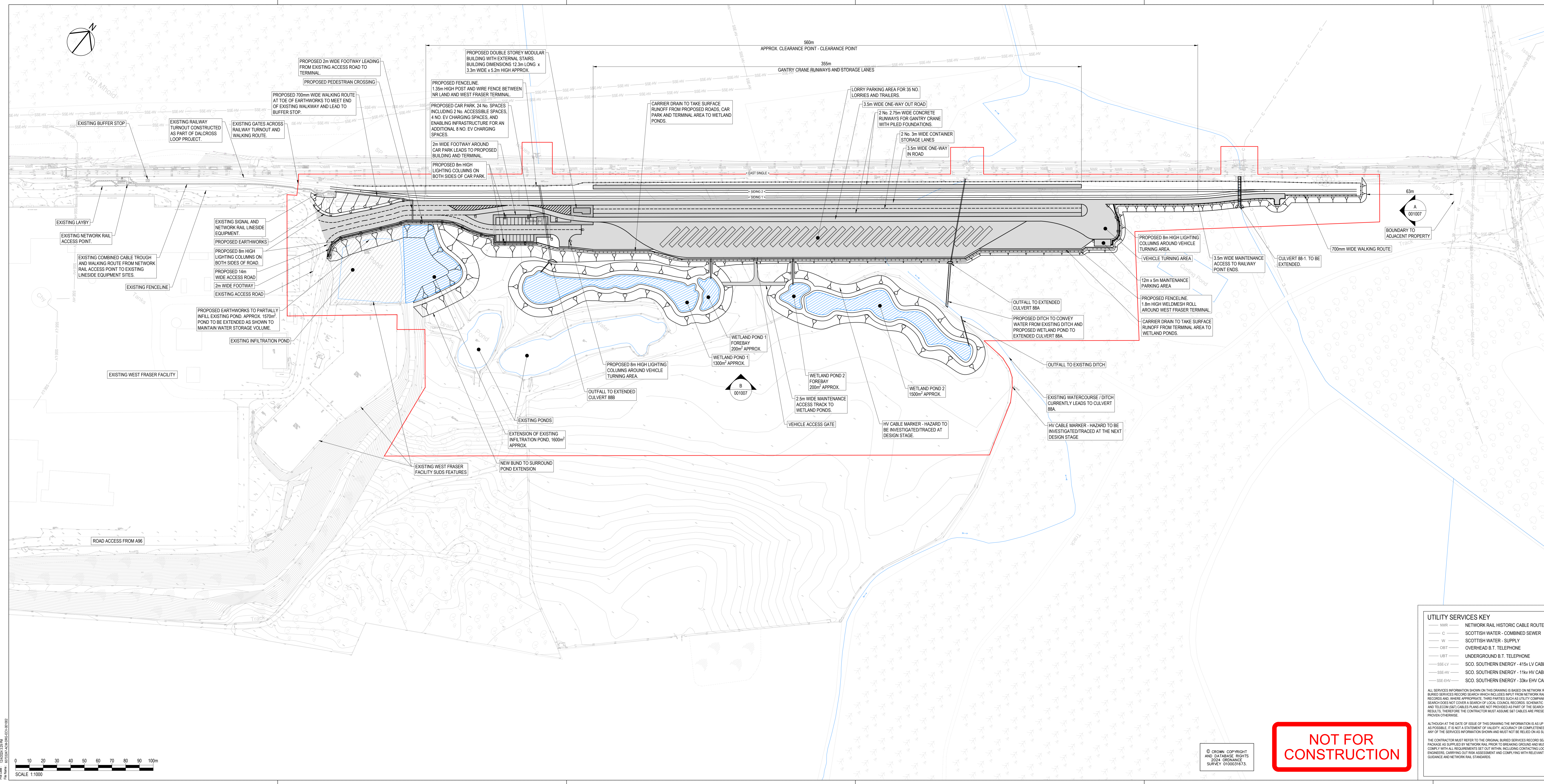
Floor Area 29.60 m²
Ceiling Height 2.3 m

Window Key
F = Fixed
S = Sliding

Portakabin®

Portakabin (Scotland) Limited
Whistleberry Road Hamilton ML3 0EJ
+44 (0)1698 713320
portakabin.co.uk

Status:	S0				
Purpose Of Issue:	Preliminary				
Client:	John G Russell				
Project:	Gatehouse Accommodation				
Title:	SL102 Plan Layout				
Scale @ A3:	Date:	Drawn By:	Checked By:		
1:50	15/03/18	AMV	JS		
Project Number:	599789-100				
Originator:	Zone:	Level:	Type:	Discipline:	Number:
					B



SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

- NOTES**
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
 - DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
 - ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS OTHERWISE STATED.
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
 - PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
 - PROPOSED WORKS OVERLAP WITH APPROX. 18300M² OF WOODED AREA AS PER OS MAPPING.
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001001 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001003 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - EARTHWORKS CROSS SECTIONS
 - 60733247-ACM-DRG-ECV-001006 - PROPOSED SURFACING PLAN
 - 60733247-ACM-DRG-ECV-001007 - ELEVATIONS

- KEY:**
- PROPOSED WORKS
 - SURVEY / OS MAPPING
 - EXTENT OF PERMANENT WORKS
 - EXISTING / PROPOSED PONDS / WATERCOURSES

Revision Details	By	Check	Date	Status
ISSUED FOR PLANNING WITH POND ACCESS TRACK INCLUDED	LD	NM	04/12/24	P03
ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	05/07/24	P01

Purpose of Issue: **FOR PLANNING**



Client: Network Rail

Project Title: **WEST FRASER**

Drawing Title: **PROPOSED GENERAL ARRANGEMENT**

Designed	Drawn	Checked	Approved	Date
LD	LD	NM	DH	July 24

Project No.	Subsidiary	Project Manager
60733247	ANIS	David Martin

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, REPRODUCED, OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY AND DENIES ANY LIABILITY WHATSOEVER TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING, PROVIDED OTHERWISE.

AECOM Infrastructure & Environment UK Limited
177 Botolph Claydon Street
Glasgow
G2 7ER
Tel: +44 (0)141 202 2600
Fax: +44 (0)141 202 2600
www.aecom.com

Drawing Number: **60733247-ACM-DRG-ECV-001002** Rev: **P03**

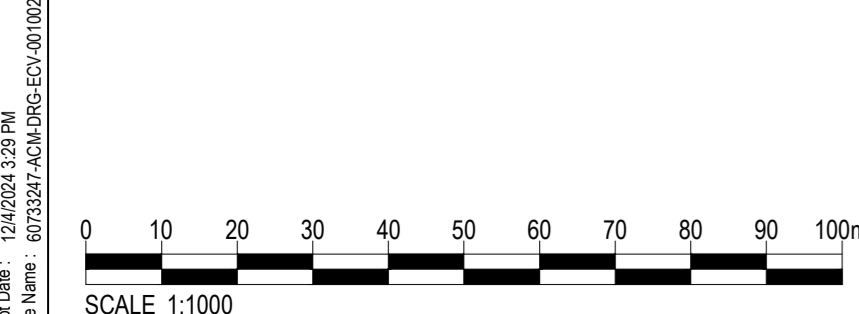
- UTILITY SERVICES KEY**
- NWR - NETWORK RAIL HISTORIC CABLE ROUTE
 - C - SCOTTISH WATER - COMBINED SEWER
 - W - SCOTTISH WATER - SUPPLY
 - OBT - OVERHEAD B.T. TELEPHONE
 - UBT - UNDERGROUND B.T. TELEPHONE
 - SSE-LV - SCO. SOUTHERN ENERGY - 415v LV CABLES
 - SSE-HV - SCO. SOUTHERN ENERGY - 11kv HV CABLES
 - SSE-EHV - SCO. SOUTHERN ENERGY - 33kv EHV CABLES

ALL SERVICES INFORMATION SHOWN ON THIS DRAWING IS BASED ON NETWORK RAIL BURIED SERVICES RECORD SEARCH WHICH INCLUDES INPUT FROM NETWORK RAIL'S RECORDS AND, WHERE APPROPRIATE, THIRD PARTIES SUCH AS UTILITY COMPANIES. THE SEARCH DOES NOT COVER A SEARCH OF LOCAL COUNCIL RECORDS, SCHEMATIC SIGNAL AND TELECOM (SIGNAL) CABLES PLANS ARE NOT PROVIDED AS PART OF THE SEARCH RESULTS. THEREFORE THE CONTRACTOR MUST ASSUME SAT CABLES ARE PRESENT UNTIL PROVEN OTHERWISE.

ALTHOUGH AT THE DATE OF ISSUE OF THIS DRAWING THE INFORMATION IS AS UP TO DATE AS POSSIBLE, IT IS NOT A STATEMENT OF VALIDITY, ACCURACY OR COMPLETENESS AS TO ANY OF THE SERVICES INFORMATION SHOWN AND MUST NOT BE RELIED ON AS SUCH. THE CONTRACTOR MUST REFER TO THE ORIGINAL BURIED SERVICES RECORD SEARCH PACKAGES AS SUPPLIED BY NETWORK RAIL PRIOR TO BREAKING GROUND AND MUST ALSO COMPLY WITH ALL REQUIREMENTS SET OUT WITHIN, INCLUDING CONTACTING LOCAL ENGINEERS, CARRYING OUT RISK ASSESSMENT AND COMPLYING WITH RELEVANT HSE GUIDANCE AND NETWORK RAIL STANDARDS.

NOT FOR CONSTRUCTION

© CROWN COPYRIGHT AND DATABASE RIGHTS 2024. ORDNANCE SURVEY 0100031673.



CDM REGULATIONS – CONSTRUCTION
SIGNIFICANT OR UNUSUAL HAZARDS ARE HIGHLIGHTED BELOW.

THERE ARE NO RISKS UNFAMILIAR TO A COMPETENT CONTRACTOR.

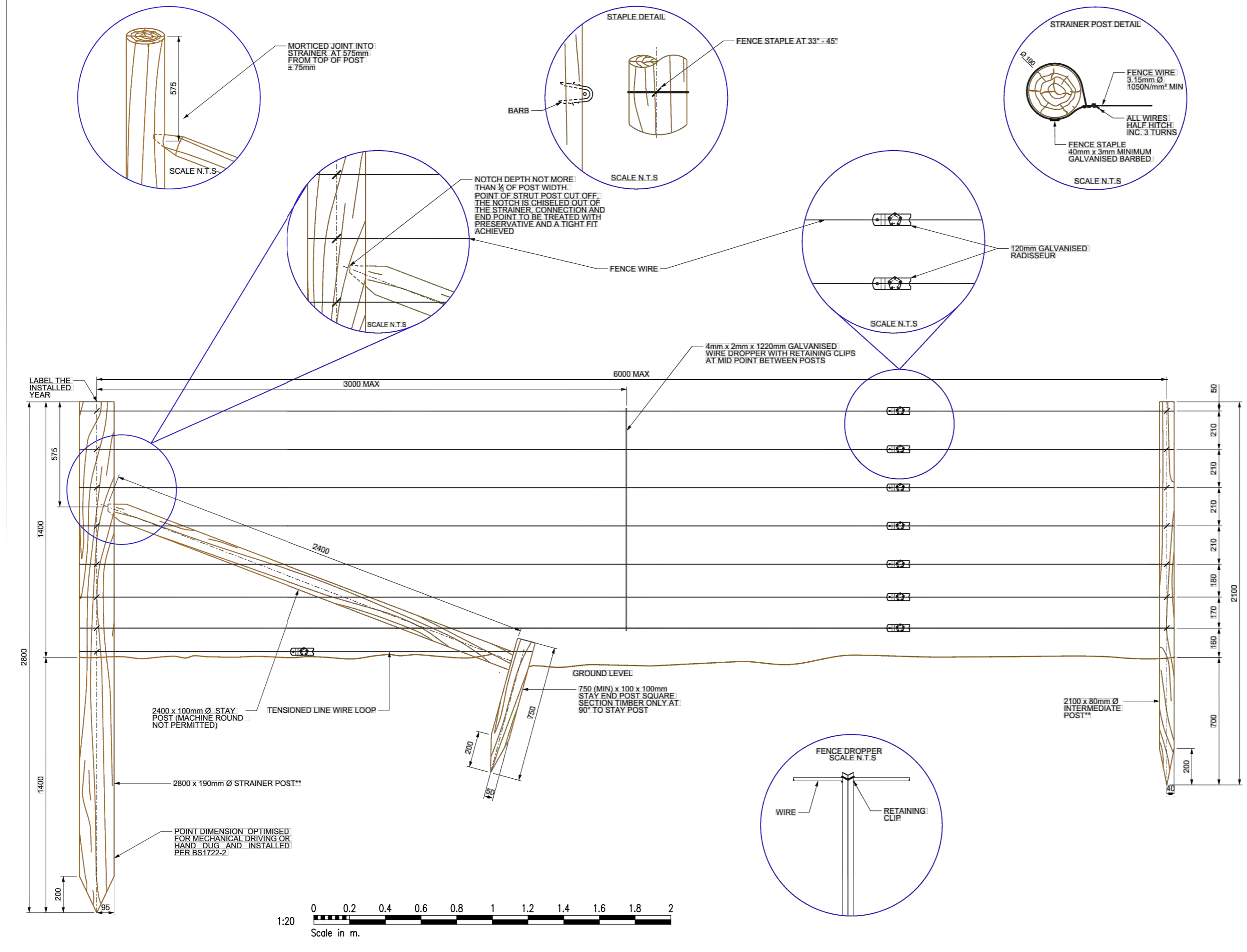
NOTE: SAFE METHODS OF WORK ARE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE IDENTIFIED IN THE HEALTH AND SAFETY FILE

NOTES:

BASED ON NETWORK RAIL STANDARD BOUNDARY FENCE DRAWING

NOTES:

- 1: ALL TIMBER COMPONENTS SHALL HAVE TRACEABLE CERTIFIED ORIGIN, AUDITABLE TREATMENT AND A STATED UNDERWRITTEN WARRANTY AS INSTALLED. (C.A. & 25 YEARS).
- 2: ALTERNATIVE POST MATERIALS ALLOWABLE TO SAME DIMENSIONS AS SHOWN ON THE DRAWING AND SAME WARRANTY.
- 3: DCL END GRAIN PRESERVATIVE TO ALL TIMBER CUTS, NOTCHES, SCORING OR DAMAGE.
- 4: Ø = MIN. DIAMETER - MACHINED ROUNDS +20mm AND CHAMFERED TOP EDGES.
- 5: CONCRETE FOUNDATIONS ONLY IF AUTHORISED, SHALL BE IN ACCORDANCE WITH BS1722-2 AND DESIGNED TO SUIT SITE SPECIFIC ENVIRONMENTAL CONSTRAINTS, SOIL AND WEATHER CONDITIONS.
- 6: FIXINGS DEPLOYED AS DETAILED.
- 7: SPECIFICATION DRAWING OR COMPONENT DEVIATIONS REQUIRE PRE-INSTALLATION APPROVAL (IN WRITING FROM THE EMPLOYERS REPRESENTATIVE).
- 8: STAPLES SHALL BE OFFSET BETWEEN 33° AND 45° THEN DRIVEN INTO POST LEAVING A 2mm TO 5mm 'RUNNING FIT' OVER WIRES.
- 9: ALL WIRES RETURNED AND STAPLED AROUND END STAINERS PER PLAN DETAIL - HIGH TENSILES HALF HITCH, HT SPIRAL CONNECTOR OR 'GRIPPLE' CLIP TO THE OFF. EACH LINE WIRE, ALL WIRES STRAINED TO 100% ± 20%.
- 10: STRAINER POST TO BE MACHINE DRIVEN OR HAND DUG AND INSTALLED PER BS1722-2.
- 11: 3.15mm HT GALVANISED LINE WIRE 40 x 3mm MINIMUM GALVANISED BARBED STAPLES.
- 12: TO BE READ AND USED ALONGSIDE MANDATORY COMPONENT SPECIFICATION DOCUMENT (NETWORK RAIL SPEC) AND BOUNDARY MEASURE MANAGEMENT MANUAL (NRL 20100100). THESE DOCUMENTS OUTLINE REQUIREMENTS AND EXCLUSIONS FOR THE SPECIFIC FENCING TYPE.
- 13: NUMBER, PLACEMENT AND LAYOUT OF STRAINING POSTS SHOULD BE IN ACCORDANCE WITH BS1722-2.



S2	P01	24/01/25	BTB	DRAWN TO SCALE, WITH SCALE BAR	SC	BTB
SUT.	REV.	DATE:	BY:	DESCRIPTION	CHK/APPD	

BT Bell
consulting engineers

Hexham Business
Park Burn Lane
HEXHAM NE46 3RU
Tel: 01434 609624
mail@btbell.co.uk
www.btbell.co.uk

CLIENT:
WEST FRASER

PROJECT:
INVERNESS RAIL TERMINAL
PLANNING DRAWINGS

TITLE:
NETWORK RAIL BOUNDARY FENCE
CLASS 111 HIGH TENSION
TIMBER POST AND 7 WIRE FENCE

DATE:	SCALE:	DESIGNED BY:	DRAWN BY:	CHECKED BY:	APPROVED BY:	SHEET
24/01/25	1:20	BTB	BTB	SC	BTB	A2

THIS DRAWING HAS BEEN CHECKED TO DESIGN CHECK LEVEL (DCL) 2 IN ACCORDANCE WITH BT BELL STRUCTURAL CHECKING PROCEDURE.

FILE NAME:	ORIGINATOR PROJECT NUMBER DESIGNATION: 24078	SUITABILITY:
PROJECT	ORIGINATOR SYSTEM LEVEL TYPE ROLE NUMBER	S2
24078-BTB-00-ZZ-DR-C-100		REVISION: P01

CDM REGULATIONS – CONSTRUCTION

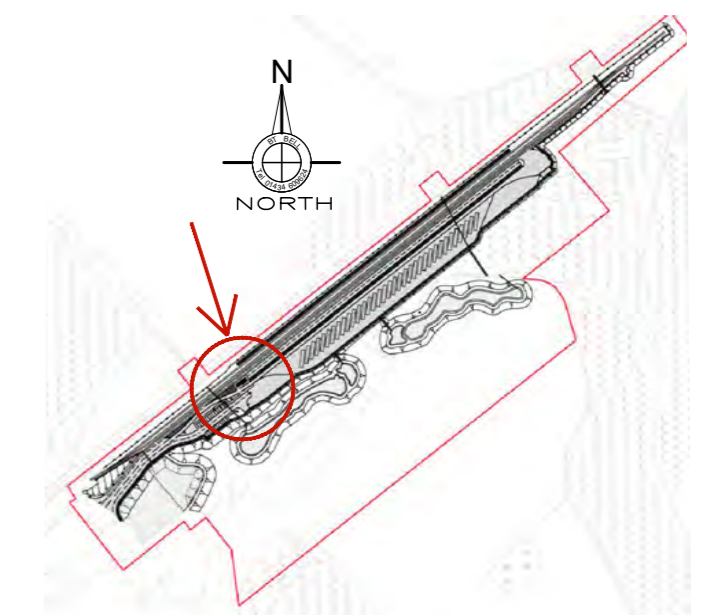
SIGNIFICANT OR UNUSUAL HAZARDS ARE HIGHLIGHTED BELOW.

THERE ARE NO RISKS UNFAMILIAR TO A COMPETENT CONTRACTOR.

NOTE: SAFE METHODS OF WORK ARE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE IDENTIFIED IN THE HEALTH AND SAFETY FILE

COLOURS

- 1) EXTERNAL WALLS, ROOF IN PLASTISOL-COATED GALVANISED STEEL COLOUR – WHITE RAL 9003
- 2) DOORS AND WINDOW FRAMES IN PLASTISOL COATED GALVANISED STEEL –COLOUR BLUE RAL 5002



LOCATION PLAN

S2	P01	28/01/25	BTB	DRAWN TO SCALE , WITH SCALE BAR	SC	BTB
SUIT.	REV.	DATE:	BY	DESCRIPTION	CHK/APD	

BT Bell
consulting engineers

Hexham Business
Park Burn Lane
HEXHAM NE46 3RU
Tel: 01434 609624
mail@btbell.co.uk
www.btbell.co.uk

CLIENT:
WEST FRASER

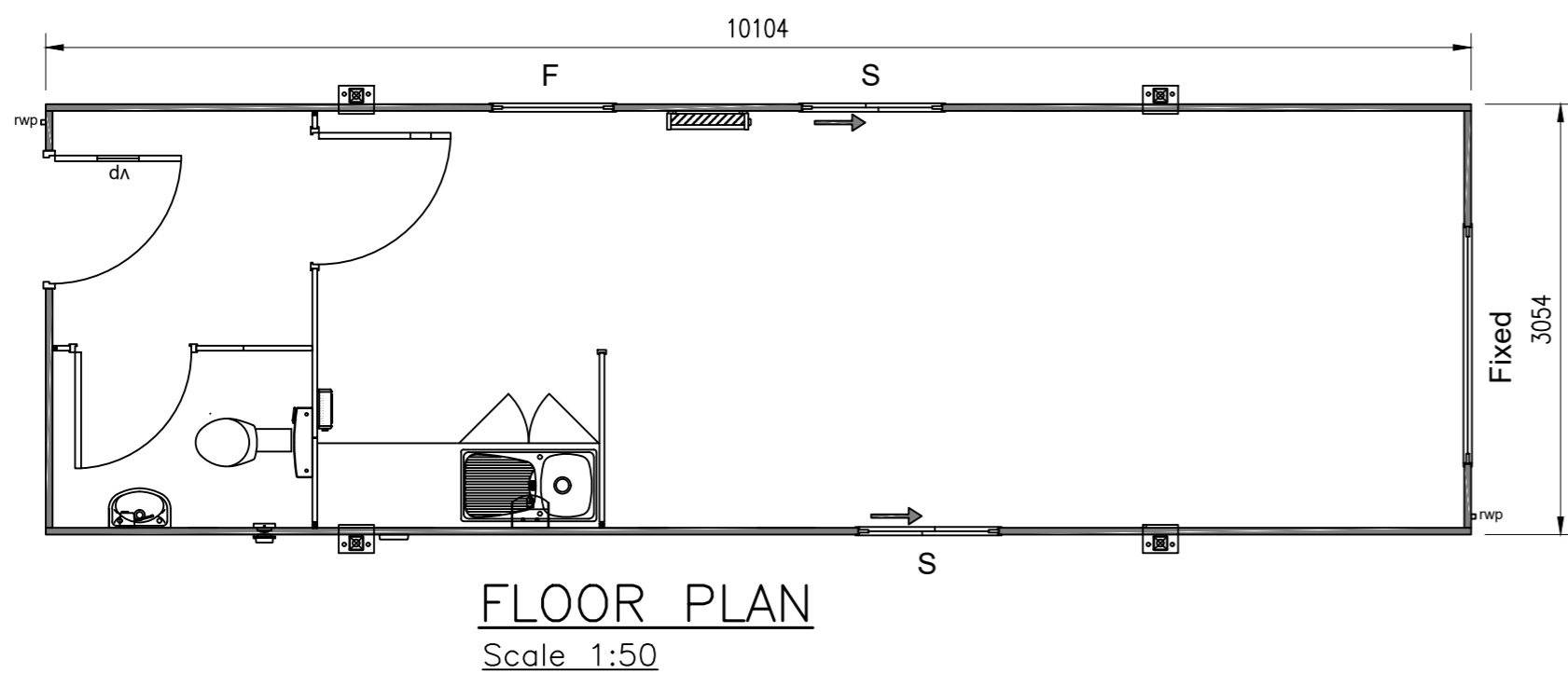
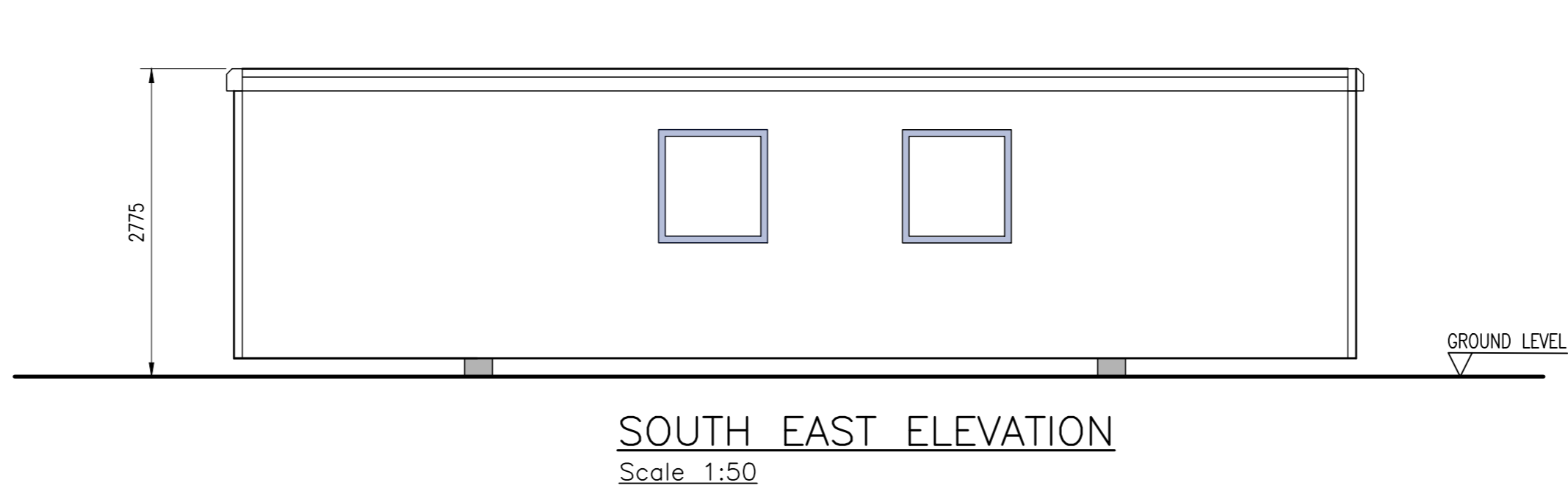
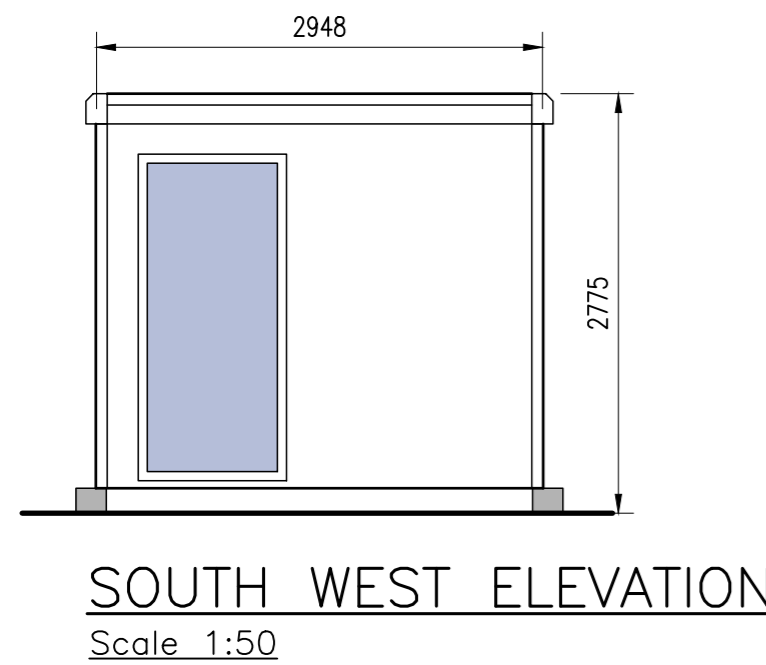
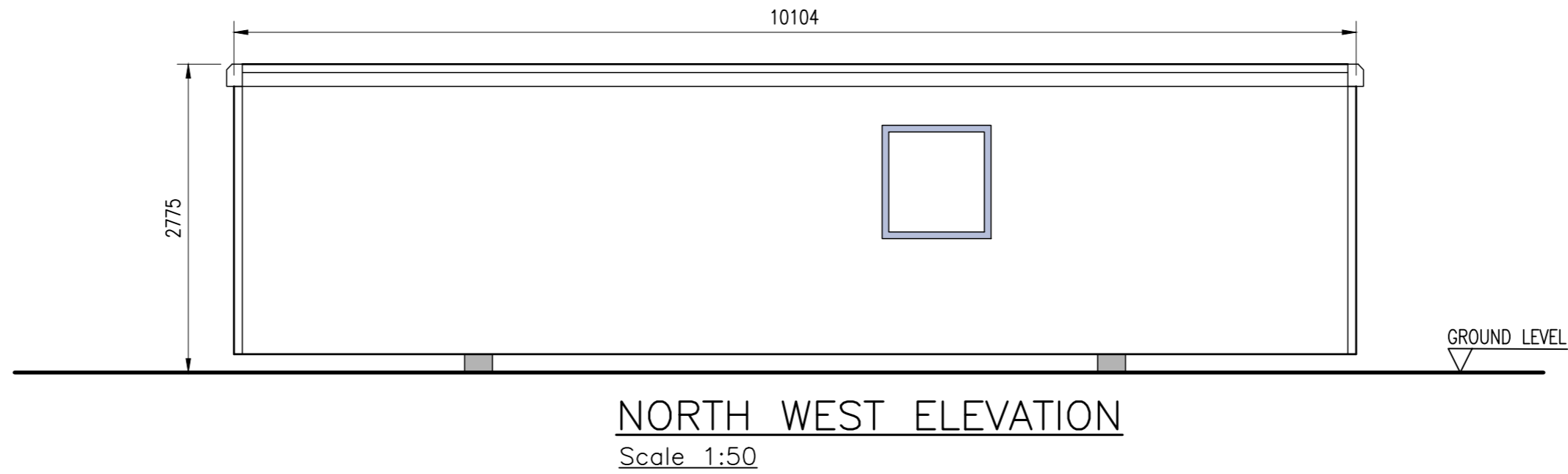
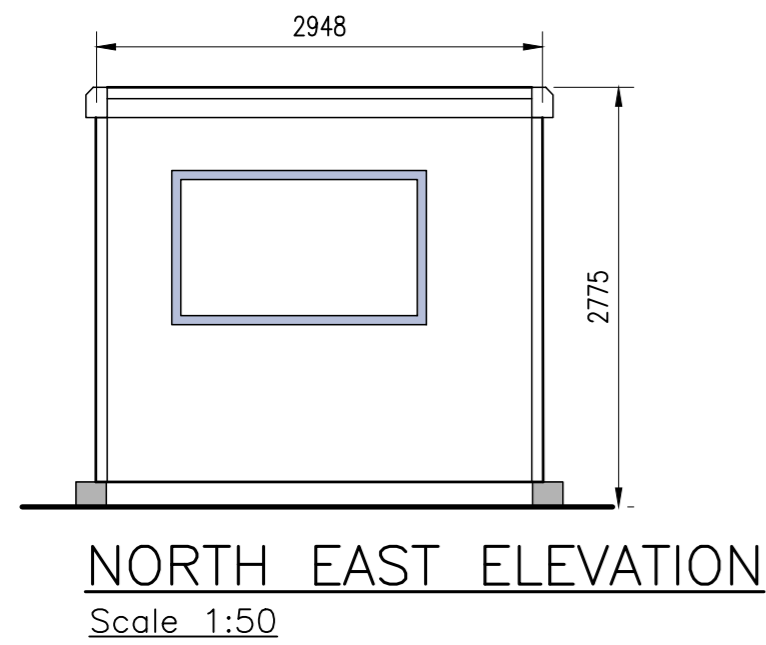
PROJECT:
INVERNESS RAIL TERMINAL
PLANNING DRAWINGS

TITLE:
GATEHOUSE
PORTAKABIN SOLUS 102

DATE:	SCALE:	DESIGNED BY:	DRAWN BY:	CHECKED BY:	APPROVED BY:	SHEET
28/01/25	1:50	BTB	BTB	SC	BTB	A2

THIS DRAWING HAS BEEN CHECKED TO DESIGN CHECK LEVEL (DCL) 2 IN ACCORDANCE WITH BT BELL STRUCTURAL CHECKING PROCEDURE.

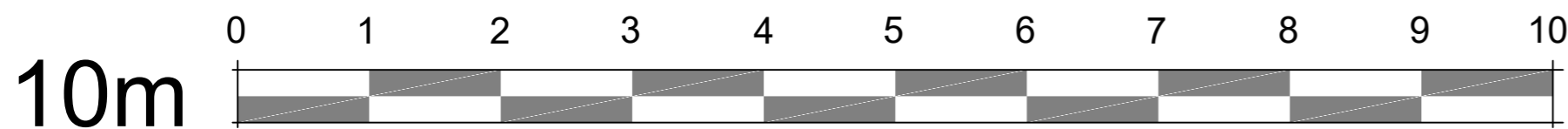
FILE NAME:	ORIGINATOR PROJECT NUMBER DESIGNATION: 24078	SUITABILITY:
PROJECT	ORIGINATOR SYSTEM LEVEL TYPE ROLE NUMBER	S2
24078-BTB-00-ZZ-DR-C-101		REVISION: P01



PORTAKABIN QUALITY

<p>Walls</p> <p>External walls are of a durable one-piece construction. High-performance, low-maintenance, plastisol-coated, galvanised steel external wall finish offers protection against fire, extreme weather and accidental damage. Internally, the walls are polyester-coated galvanised steel, providing an impact resistant, wipe-clean finish.</p> <p>Floor</p> <p>The floor has a rigid galvanised steel frame and an aluminium-zinc coated steel sheet underdrawing, which offers a sturdy and robust feel. The design also prevents infestation as the thermal insulation material is laid on top of the underdrawing. The suspended floor is completely integrated into the structure of the building. This enables the building to be sited close to the ground for one-step easy access. A variety of floor coverings are available depending on the application of the building, including hard-wearing vinyl or carpeting.</p>	<p>Roof</p> <p>Thermal insulation is provided by the construction of the roof. The roof deck is of one-piece construction and is impact resistant. It is covered with solar-reflective, profiled plastisol-coated, galvanised steel to reduce heat gain. Internally, the ceiling is designed to prevent condensation problems. It is covered with the same polyester steel as the walls for a low-maintenance finish.</p> <p>Doors</p> <p>Steel-faced doors have an insulating core that minimises heat loss. The anodised aluminium frame also incorporates draught proof weather seals. Protection against theft and vandalism is given by a cylinder mortice lock, which has escape override on the inside. A low-level threshold improves access and can help you to comply with the Equality Act 2010.</p>	<p>Windows</p> <p>Both the fixed and opening have distinctive square cornered aluminium frames and polyester powder coated paint finish. Opening windows have lockable handles with keys. The windows are fully double-glazed for excellent thermal and acoustic performance. Tinted glazing reduces solar glare and a thermal break prevents condensation problems.</p> <p>Building layout</p> <p>A variety of room layouts can be achieved by the installation of partition walls, with a matching polyester steel finish. Portakabin Solus buildings can be linked to form larger complexes or stacked to form two storey buildings.</p> <p>Foundations</p> <p>Portakabin Solus buildings are quickly and easily sited. The need for pad foundations is dependent on site conditions, total weight of the building and the imposed loads. Advice on foundation loads and positions will be given by Portakabin staff.</p>	<p>Design loads</p> <p>Floor – 3.00kN/m² Roof – 0.75kN/m²</p> <p>Thermal insulation</p> <p>The walls, doors and roof are insulated with environmentally friendly CFC-free Envirofoam: a rigid polymer insulation material that is lightweight and yet enhances long-life structural strength and ensures an energy-efficient building.</p> <p>Insulation values</p> <p>Fabric U-values (W/m²·K) Floor 0.35 Walls 0.59 Roof 0.42</p>
--	---	---	---

SL102 Solus Building



Technical Information

3rd Angle

Design

Standard: BSEN40, CD354 & Excel Tender (25 years)
 10 minute mean wind velocity: 29.5m/s
 Maximum altitude 83m
 Terrain category I
 Load class B
 Deflection class 2

Loading

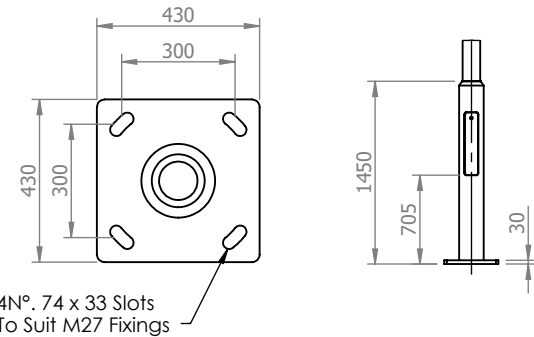
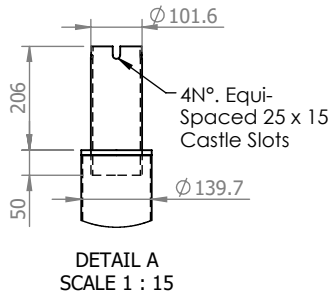
Post top or side entry lantern 21.5kg/0.2m²
 Sign 10kg/0.3m² mounted at 2.5m above G.L. with 400mm offset
 Sign lights 4kg/0.045m² each mounted at 3m above G.L.

Finish

Galvanised to BSEN 1461:2009 & glass flake root internally and externally up to 250mm above G.L.

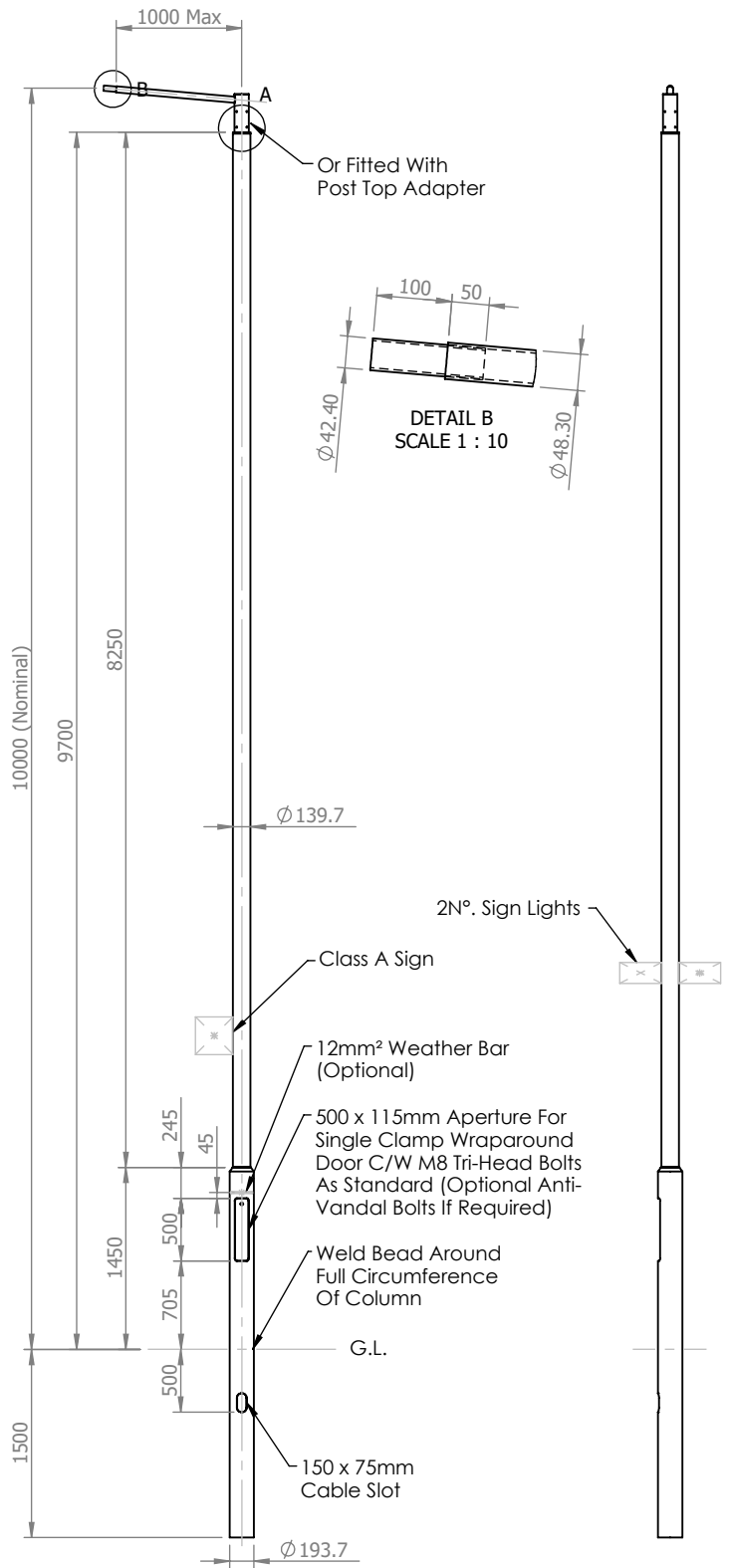
Welding

In accordance with BSEN1011
 Procedures to BSEN ISO 15614-1:2017
 Welders qualified to BSEN ISO 9606-1:2017



**Flange Plated Option
SCALE 1:20**

Additional Technical Data	
Lot 3 Item No.	Scotland Excel Product Description
SLMSCOLUMN021	10 Metre column
SLMSCOLUMN022	Extra over item for the provision of flanged base
SLMSCOLUMN023	Extra over item for water shed bar
SLMSCOLUMN024	Extra over item for additional tamperproof lock
SLMSCOLUMN032	10 Metre, complete with 0.5 metre bracket projection having 43mm od spigot and anti-rotating device
SLMSCOLUMN033	10 Metre, complete with 0.75 metre bracket projection having 43mm od spigot and anti-rotating device
SLMSCOLUMN034	10 Metre, complete with 1.0 metre bracket projection having 43mm od spigot and anti-rotating device



2022 © Copyright



20-22 Abbotsinch Road, Grangemouth, FK3 9UX
 www.streetlightingsupplies.com
 Tel. 01324 665 602

Notes
 All dimensions in millimetres unless stated otherwise.
 When Multiple views are shown 3rd Angle Projection is assumed.

Revisions		
Rev.	Description	Date

Title
 10m Root Mounted Tubular Column

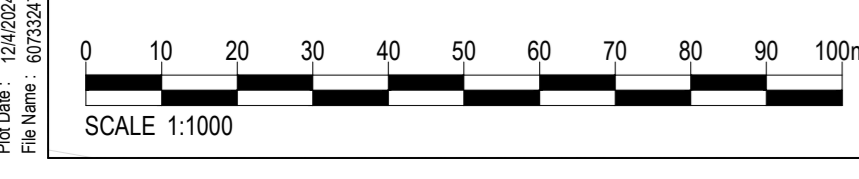
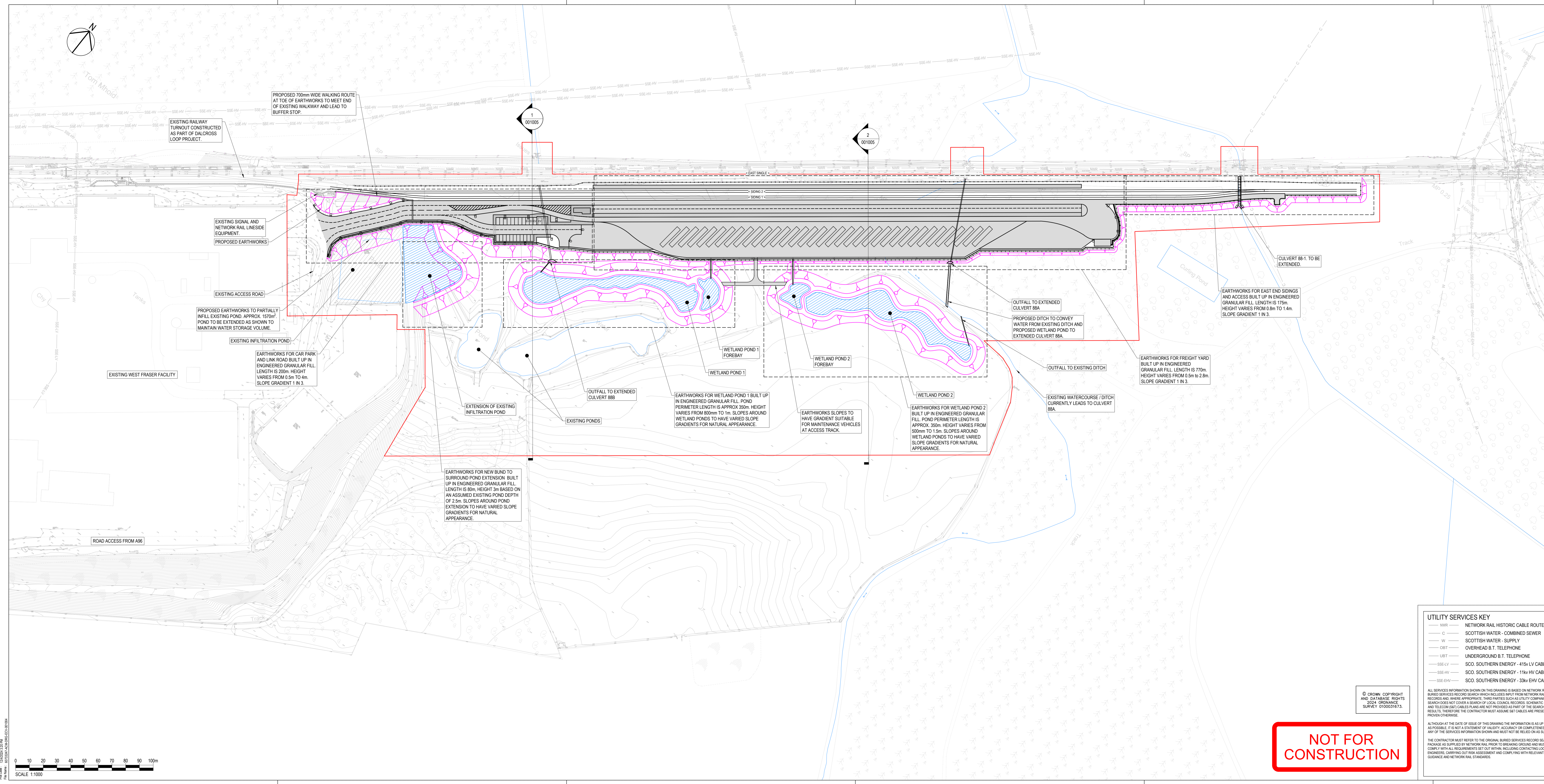
Drg No. 46989-0 Scale:1:60
Sheet 1 of 1

Drawn By: TS **Chk'd:** DA

Ref. General Arrangement

Contract:

Customer: **Date:** 08/06/2022



SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
- DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
- ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS IDENTIFIED OTHERWISE.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
- PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001001 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001002 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001003 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - EARTHWORKS CROSS SECTIONS
 - 60733247-ACM-DRG-ECV-001005 - PROPOSED SURFACING PLAN
 - 6073247-ACM-DRG-ECV-001000 - ELEVATIONS

KEY:

PROPOSED WORKS

SURVEY / OS MAPPING

EXTENT OF PERMANENT WORKS

EXISTING / PROPOSED PONDS / WATERCOURSES

PROPOSED EARTHWORKS

EXTENT OF EARTHWORKS AREA

Revision Details	LD	NM	Date	Suffix
REISSUED FOR PLANNING WITH POND ACCESS TRACK INCLUDED	LD	NM	04/12/24	P03
ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	19/07/24	P01

Purpose of Issue: **FOR PLANNING**



Client: **Network Rail**

Project Title: **WEST FRASER**

PROPOSED EARTHWORKS PLAN

Designed	Drawn	Checked	Approved	Date
LD	LD	NM	DH	July 24

AECOM Internal Project No: 60733247
 Scale @ 25x1/251
 Zone/UTM/Zone: AN3
 1:1000

Project Manager: David Martin
 Subality: David Martin
 Zone/UTM/Zone: AN3
 1:1000

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, REPRODUCED, REPRODUCED OR HELD UP BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING, PROVIDED OTHERWISE.

AECOM Infrastructure & Environment UK Limited
 177 Botwell Street
 Glasgow
 G2 2ER
 Tel: +44 (0)141 202 2600
 Fax: +44 (0)141 202 2600
 www.aecom.com

Drawing Number: **60733247-ACM-DRG-ECV-001004** Rev: **P03**

NOT FOR CONSTRUCTION

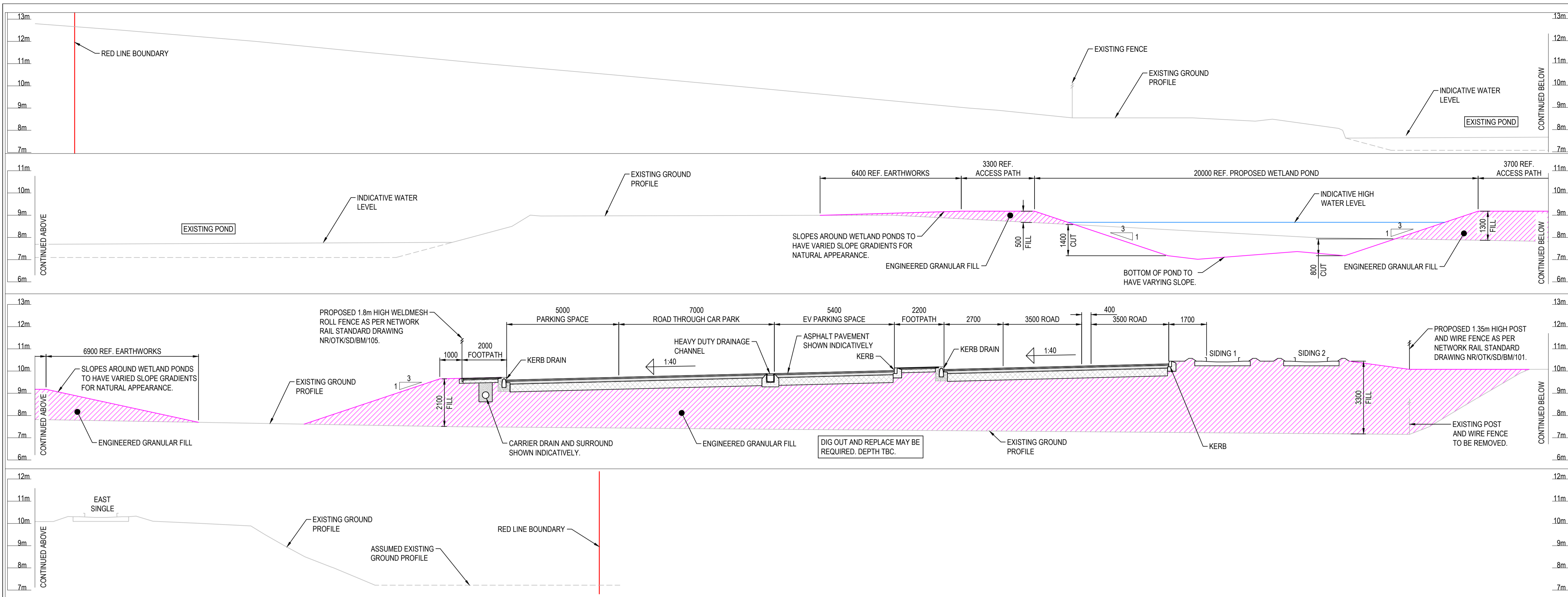
UTILITY SERVICES KEY

- NWR NETWORK RAIL HISTORIC CABLE ROUTE
- C SCOTTISH WATER - COMBINED SEWER
- W SCOTTISH WATER - SUPPLY
- OBT OVERHEAD B.T. TELEPHONE
- UBT UNDERGROUND B.T. TELEPHONE
- SSE-LV SCO. SOUTHERN ENERGY - 415v LV CABLES
- SSE-HV SCO. SOUTHERN ENERGY - 11kv HV CABLES
- SSE-EHV SCO. SOUTHERN ENERGY - 33kv HV CABLES

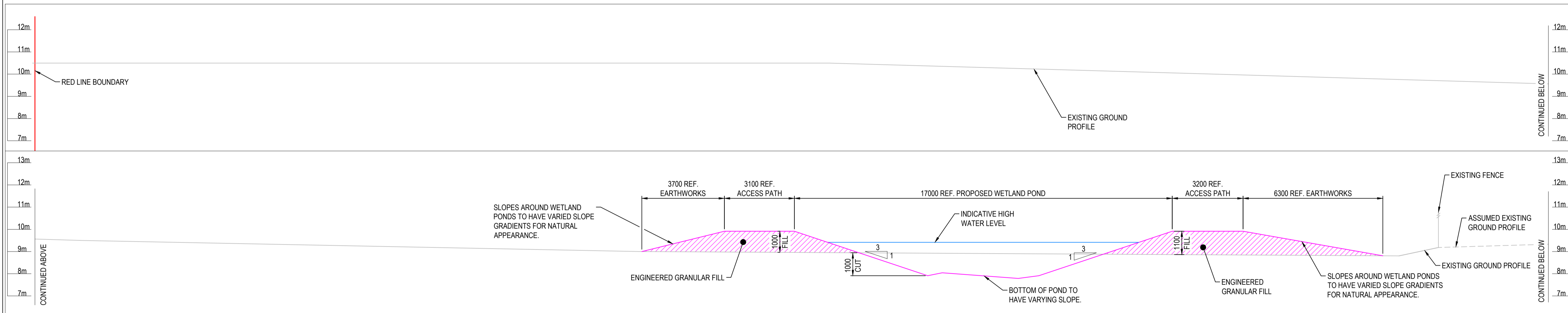
ALL SERVICES INFORMATION SHOWN ON THIS DRAWING IS BASED ON NETWORK RAIL BURIED SERVICES RECORD SEARCH WHICH INCLUDES INPUT FROM NETWORK RAIL'S RECORDS AND, WHERE APPROPRIATE, THIRD PARTIES SUCH AS UTILITY COMPANIES. THE SEARCH DOES NOT COVER A SEARCH OF LOCAL COUNCIL RECORDS, SCHEMATIC SIGNAL AND TELECOM (SIGNAL) PLANS ARE NOT PROVIDED AS PART OF THE SEARCH. RESULTS THEREFORE THE CONTRACTOR MUST ASSUME SAT CABLES ARE PRESENT UNTIL PROVEN OTHERWISE.

ALTHOUGH AT THE DATE OF ISSUE OF THIS DRAWING THE INFORMATION IS AS UP TO DATE AS POSSIBLE, IT IS NOT A STATEMENT OF VALIDITY, ACCURACY OR COMPLETENESS AS TO ANY OF THE SERVICES INFORMATION SHOWN AND MUST NOT BE RELIED ON AS SUCH. THE CONTRACTOR MUST REFER TO THE ORIGINAL BURIED SERVICES RECORD SEARCH PACKAGES AS SUPPLIED BY NETWORK RAIL PRIOR TO BREAKING GROUND AND MUST ALSO COMPLY WITH ALL REQUIREMENTS SET OUT WITHIN, INCLUDING CONTACTING LOCAL ENGINEERS, CARRYING OUT RISK ASSESSMENT AND COMPLYING WITH RELEVANT HSE GUIDANCE AND NETWORK RAIL STANDARDS.

File Name: 60733247-ACM-DRG-ECV-001004.dwg
 Plot Date: 2024/07/24 10:00:00
 Plotter: HP DesignJet T1100



SECTION 1
(001004)



SECTION 2
(001004)

SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
- DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
- ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS DEFINED OTHERWISE.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
- PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001001 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001002 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001003 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - PROPOSED SURFACING PLAN
 - 60733247-ACM-DRG-ECV-001007 - ELEVATIONS

KEY:

PROPOSED WORKS _____

SURVEY _____

ASSUMED GROUND PROFILE (TOPO SURVEY NOT AVAILABLE) _____

EXTENT OF PERMANENT WORKS _____

PROPOSED EARTHWORKS _____

NOT FOR CONSTRUCTION

Revision Details	By	Check	Date	Suffix
ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	19/07/24	P01

Purpose of issue: **FOR PLANNING**



Client: **Network Rail**

Project Title: **WEST FRASER**

Drawing Title: **EARTHWORKS CROSS SECTIONS**

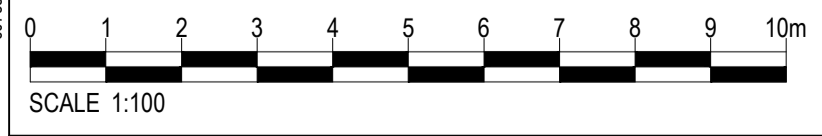
Designed LD	Drawn LD	Checked NM	Approved DH	Date
60733247				July 24
AECOM Internal Project No. 60733247		Subsidiary -	Project Manager David Martin	
Scale @ A1 1:100		Zone/ELRM/Stage ANI 3	david.martin@aecom.com 0141 202 0500	

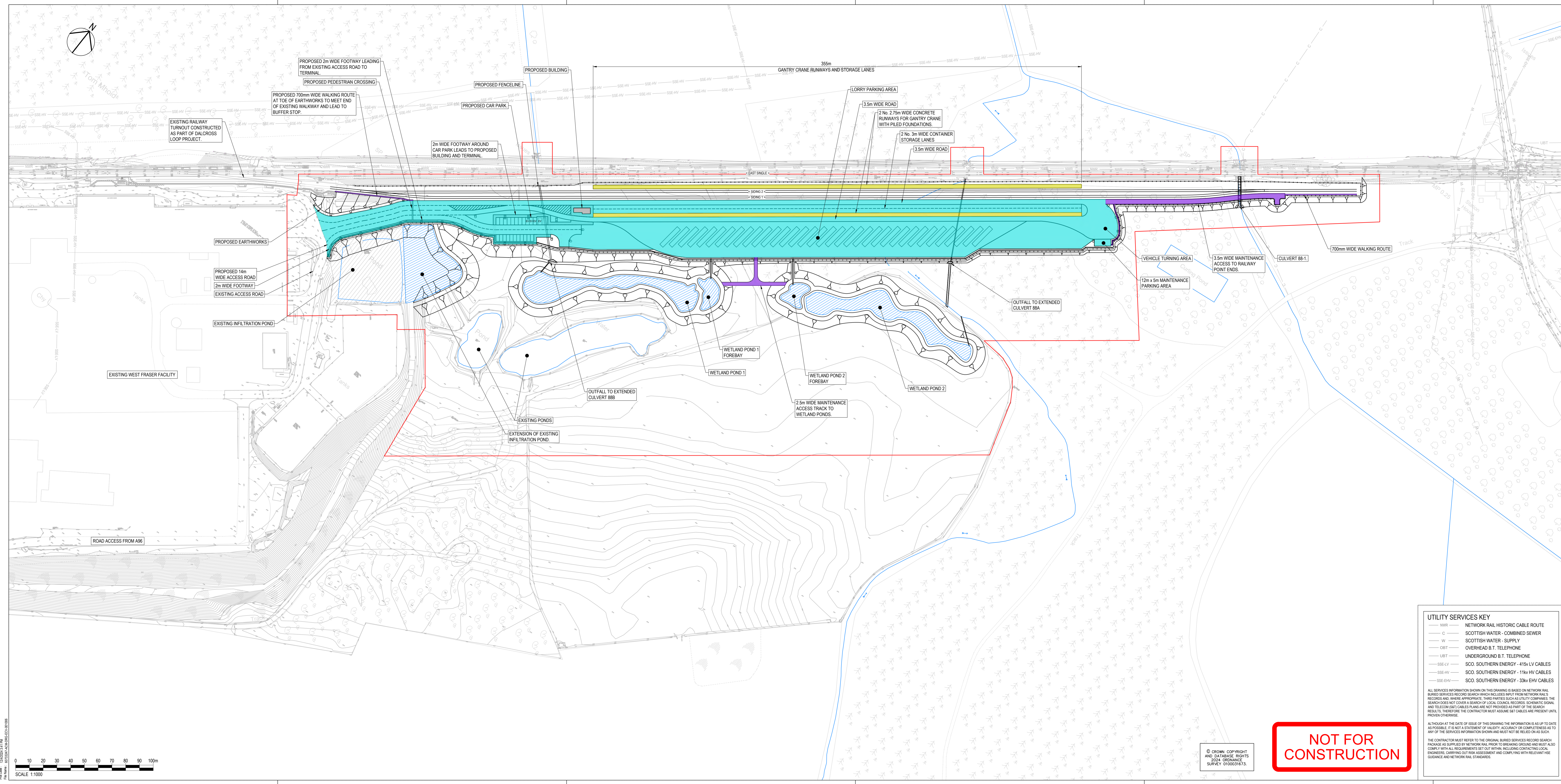
THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, MODIFIED, REPRODUCED OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING.

AECOM Infrastructure & Environment UK Limited
177 Bothwell Street
Glasgow
G2 7ER
Tel: +44 (0)141 202 0500
Fax: _____
www.aecom.com

Drawing Number: **60733247-ACM-DRG-ECV-001005** Rev: **P02**

Plot Date: 8/15/2024 4:13 PM
 File Name: 60733247-ACM-DRG-ECV-001005





SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION BOX

IT IS ASSUMED THAT ALL WORKS ON THIS DRAWING WILL BE CARRIED OUT BY A COMPETENT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN APPROPRIATE METHOD STATEMENT.

THIS DRAWING IS TO BE USED ONLY FOR THE PURPOSE OF ISSUE THAT IT WAS ISSUED FOR AND IS SUBJECT TO AMENDMENT.

NOTES

- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTATION.
- DO NOT SCALE FROM THIS DRAWING. USE ONLY PRINTED DIMENSIONS.
- ALL DIMENSIONS IN MILLIMETRES. ALL CHANGES, LEVELS AND COORDINATES ARE IN METRES UNLESS OTHERWISE STATED.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE PROJECT HEALTH & SAFETY FILE FOR ANY IDENTIFIED POTENTIAL RISKS.
- PROPOSALS SHOWN ARE BASED ON WORK IN PROGRESS PRELIMINARY DESIGNS AND MAY CHANGE DURING DESIGN DEVELOPMENT.
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE FOLLOWING DRAWINGS:
 - 60733247-ACM-DRG-ECV-001000 - LOCATION PLAN
 - 60733247-ACM-DRG-ECV-001001 - PROPOSED GENERAL ARRANGEMENT
 - 60733247-ACM-DRG-ECV-001002 - SITE PLAN
 - 60733247-ACM-DRG-ECV-001004 - PROPOSED EARTHWORKS PLAN
 - 60733247-ACM-DRG-ECV-001005 - DRAINAGEWORKS CROSS SECTIONS
 - 6073247-ACM-DRG-ECV-001000 - ELEVATIONS

KEY:

PROPOSED WORKS
 SURVEY / OS MAPPING
 EXTENT OF PERMANENT WORKS
 EXISTING / PROPOSED PONDS / WATERCOURSES

SURFACING KEY:

ASPHALT SURFACING
 UNBOUND SURFACING
 CONCRETE PILECAP

REISSUED FOR PLANNING WITH POND ACCESS TRACK INCLUDED	LD	NM	04/12/24	P03
ISSUED FOR PLANNING	LD	NM	16/08/24	P02
ISSUED FOR COMMENT	LD	NM	19/07/24	P01

Revision Details

Rev	Check	Date	Suffix

Purpose of Issue: **FOR PLANNING**

Client: **Network Rail**

Project Title: **WEST FRASER**

Drawing Title: **PROPOSED SURFACING PLAN**

Designed	Drawn	Checked	Approved	Date
LD	LD	NM	DH	July 24

AECOM Internal Project No: 60733247
 Scale @ 25x1/251
 1:1000

Subsidiary: ANIS
 Zone/UTM/Map: ANIS
 1:141 202 2600

Project Manager: David Martin
 david.martin@aecom.com
 1:141 202 2600

THIS DRAWING HAS BEEN PREPARED FOR THE USE OF AECOM'S CLIENT. IT MAY NOT BE USED, MODIFIED, REPRODUCED OR RELIED UPON BY THIRD PARTIES, EXCEPT AS AGREED BY AECOM OR AS REQUIRED BY LAW. AECOM ACCEPTS NO RESPONSIBILITY, AND DENIES ANY LIABILITY WHATSOEVER, TO ANY PARTY THAT USES OR RELIES ON THIS DRAWING, PROVIDED OTHERWISE.

AECOM Infrastructure & Environment UK Limited
 177 Botolph Claydon
 Botolph Claydon
 Norfolk
 IP19 1AB
 Tel: +44 (0)141 202 2600
 Fax: +44 (0)141 202 2600
 www.aecom.com

Drawing Number: 60733247-ACM-DRG-ECV-001006
 Rev: P03

UTILITY SERVICES KEY

- NWR - NETWORK RAIL HISTORIC CABLE ROUTE
- C - SCOTTISH WATER - COMBINED SEWER
- W - SCOTTISH WATER - SUPPLY
- QBT - OVERHEAD B.T. TELEPHONE
- UBT - UNDERGROUND B.T. TELEPHONE
- SSE-LV - SCO. SOUTHERN ENERGY - 415v LV CABLES
- SSE-HV - SCO. SOUTHERN ENERGY - 11kv HV CABLES
- SSE-EHV - SCO. SOUTHERN ENERGY - 33kv EHV CABLES

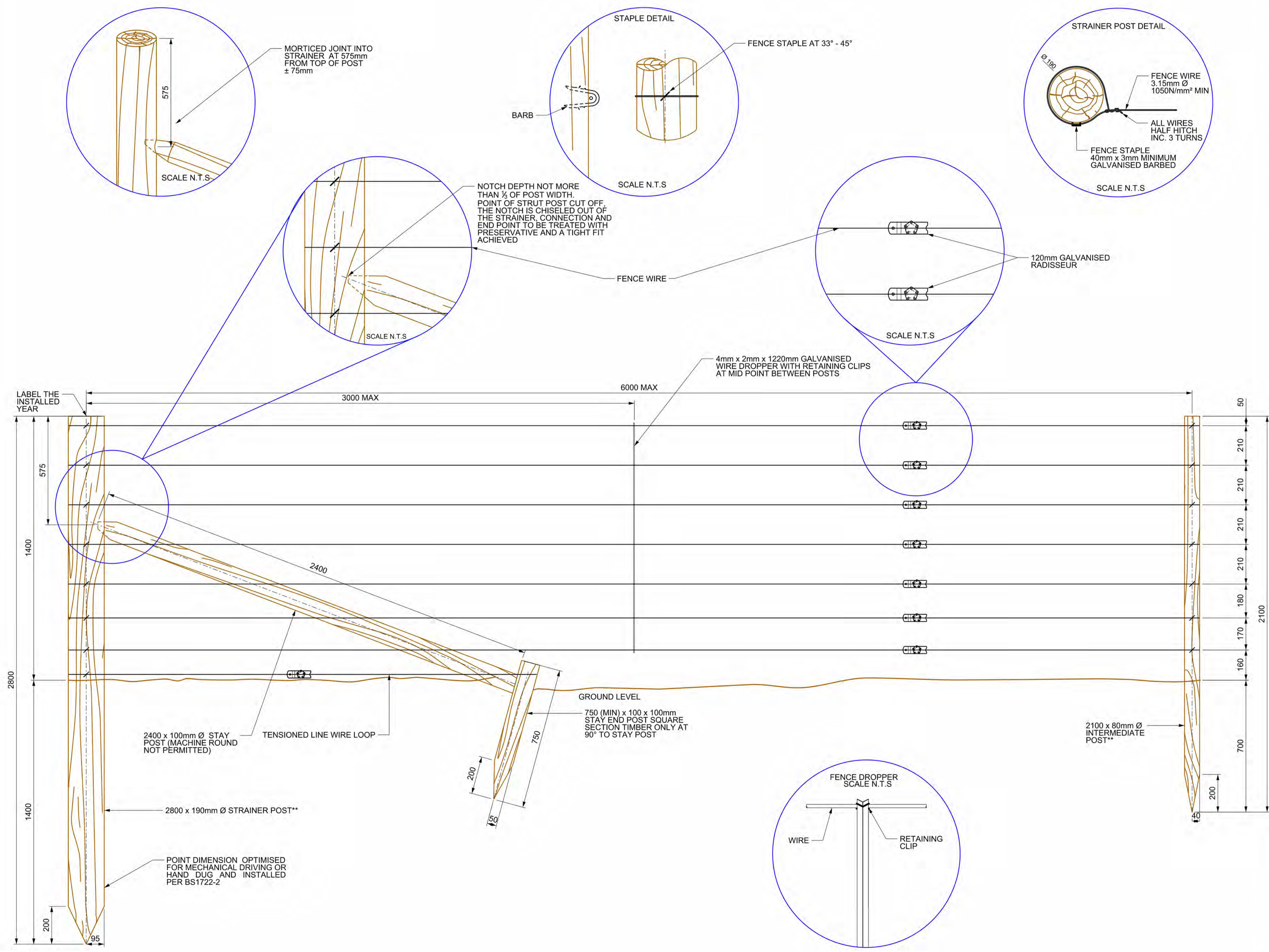
ALL SERVICES INFORMATION SHOWN ON THIS DRAWING IS BASED ON NETWORK RAIL BURIED SERVICES RECORD SEARCH WHICH INCLUDES INPUT FROM NETWORK RAIL'S RECORDS AND, WHERE APPROPRIATE, THIRD PARTIES SUCH AS UTILITY COMPANIES. THE SEARCH DOES NOT COVER A SEARCH OF LOCAL COUNCIL RECORDS, SCHEMATIC SIGNAL AND TELECOM (BT) CABLES PLANS ARE NOT PROVIDED AS PART OF THE SEARCH. RESULTS THEREFORE THE CONTRACTOR MUST ASSUME SAT CABLES ARE PRESENT UNLESS PROVIDED OTHERWISE.

ALTHOUGH AT THE DATE OF ISSUE OF THIS DRAWING THE INFORMATION IS AS UP TO DATE AS POSSIBLE, IT IS NOT A STATEMENT OF VALIDITY, ACCURACY OR COMPLETENESS AS TO ANY OF THE SERVICES INFORMATION SHOWN AND MUST NOT BE RELIED ON AS SUCH.

THE CONTRACTOR MUST REFER TO THE ORIGINAL BURIED SERVICES RECORD SEARCH PACKAGES AS SUPPLIED BY NETWORK RAIL PRIOR TO BREAKING GROUND AND MUST ALSO COMPLY WITH ALL REQUIREMENTS SET OUT WITHIN, INCLUDING CONTACTING LOCAL ENGINEERS, CARRYING OUT RISK ASSESSMENT AND COMPLYING WITH RELEVANT HEALTH AND SAFETY AND NETWORK RAIL STANDARDS.

© CROWN COPYRIGHT AND DATABASE RIGHTS 2024 ORDNANCE SURVEY 0100031673.

NOT FOR CONSTRUCTION



- NOTES:
1. ALL TIMBER COMPONENTS SHALL HAVE TRACEABLE CERTIFIED ORIGIN, AUDITABLE TREATMENT AND A STATED UNDERWRITTEN WARRANTY AS INSTALLED. UC4 & 25 YEARS.
 2. ALTERNATIVE POST MATERIALS ALLOWABLE TO SAME DIMENSIONS AS SHOWN ON THE DRAWING AND SAME WARRANTY.
 3. UC4 END GRAIN PRESERVATIVE TO ALL TIMBER CUTS, NOTCHES, SCORING OR DAMAGE.
 4. Ø = MIN. DIAMETER ** - MACHINED ROUNDS +20mm AND CHAMFERED TOP EDGES.
 5. CONCRETE FOUNDATIONS ONLY IF AUTHORISED, SHALL BE IN ACCORDANCE WITH BS1722-2 AND DESIGNED TO SUIT SITE SPECIFIC ENVIRONMENTAL CONSTRAINTS, SOIL AND WEATHER CONDITIONS.
 6. FIXINGS DEPLOYED AS DETAILED.
 7. SPECIFICATION DRAWING OR COMPONENT DEVIATIONS REQUIRE PRE INSTALLATION APPROVAL (IN WRITING FROM THE EMPLOYERS REPRESENTATIVE).
 8. STAPLES SHALL BE OFFSET BETWEEN 33° AND 45° THEN DRIVEN INTO POST LEAVING A 2mm TO 5mm "RUNNING FIT" OVER WIRES.
 9. ALL WIRES RETURNED AND STAPLED AROUND END STRAINERS PER PLAN DETAIL. HIGH TENSILES HALF HITCH, HT SPIRAL CONNECTOR OR "GRIPPLE T-CLIP" TO TIE OFF EACH LINE WIRE. ALL WIRES STRAINED TO 150kg/F ±20%.
 10. STRAINER POST TO BE MACHINE DRIVEN OR HAND DUG AND INSTALLED PER BS1722-2.
 11. 3.15mm HT GALVANISED LINE WIRE 40 x 3mm MINIMUM GALVANISED BARBED.
 12. TO BE READ AND USED ALONGSIDE MANDATORY COMPONENT SPECIFICATION DOCUMENT (NR/OTK/SD/BM/101/SPEC) AND BOUNDARY MEASURE MANAGEMENT MANUAL (NR/L2/OTK/5100). THESE DOCUMENTS OUTLINE REQUIREMENTS AND EXCLUSIONS FOR THE SPECIFIC FENCING TYPE.
 13. NUMBER, PLACEMENT AND LAYOUT OF STRAINING POSTS SHOULD BE IN ACCORDANCE WITH BS1722-2.

Rev	Date	Description of Revisions	Drawn	Chkd	Appr
B01	05/02/21	ISSUE FOR APPROVAL	AM	GM	KC
A02	16/12/20	AMENDMENTS FROM REVIEW	AM	GM	KC
A01	17/07/20	ISSUE FOR APPROVAL	AM	GM	KC
P04	28/02/19	AMENDMENTS FROM REVIEW	VK		

Status PUBLISHED



Contractor(s)

Project

BOUNDARY MEASURE STANDARD DRAWING

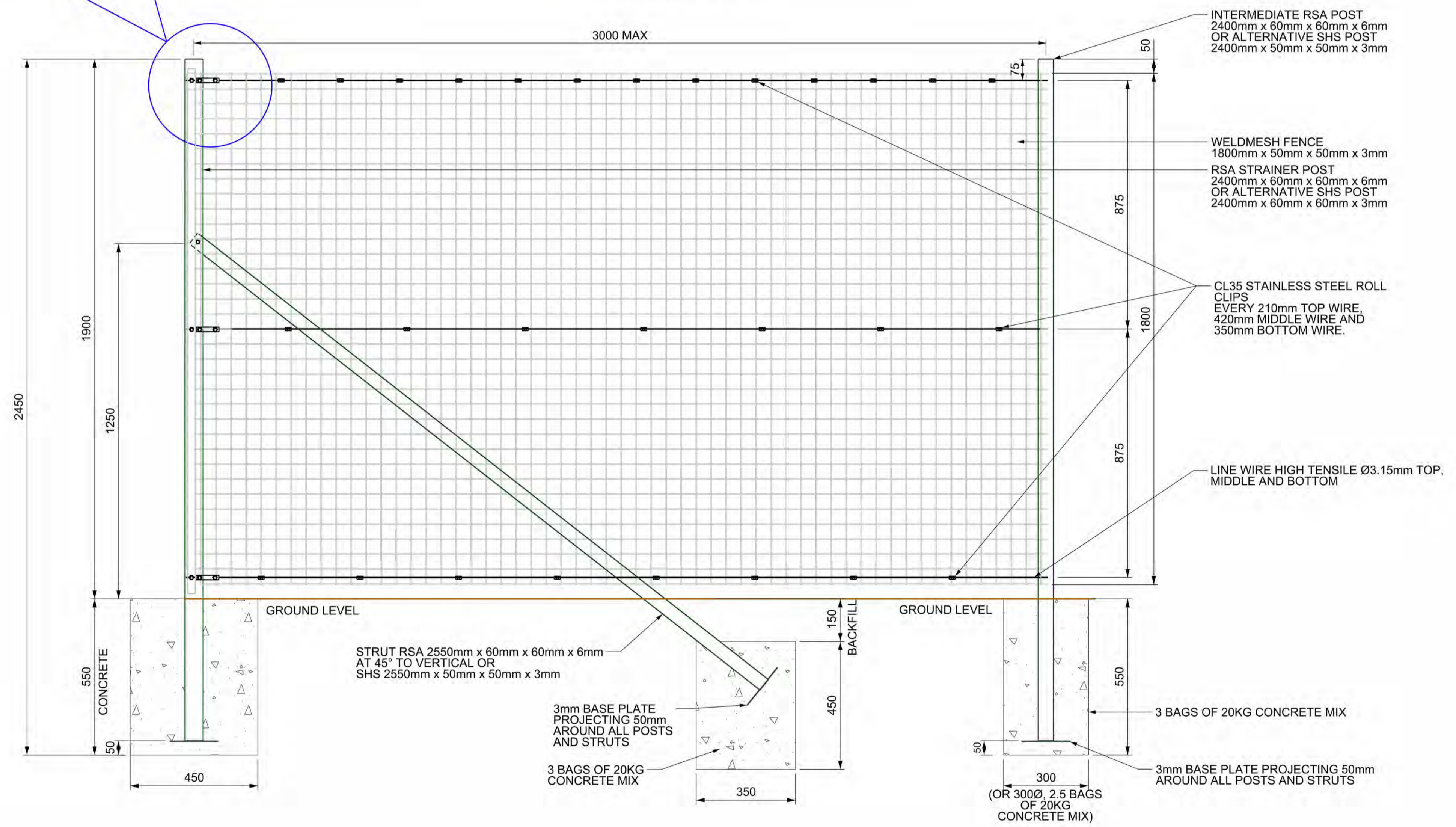
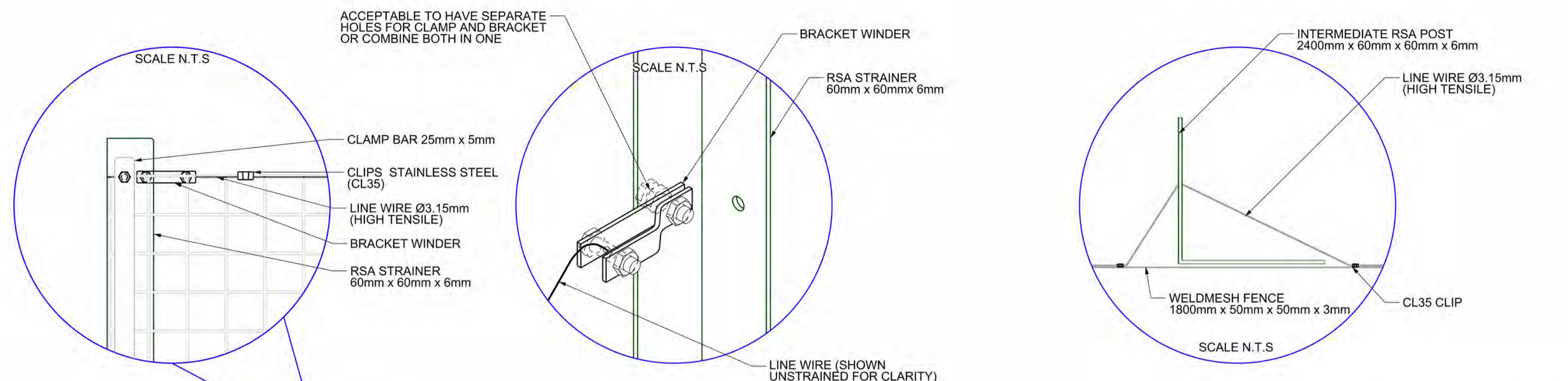
Drawing Title
CLASS III - HIGH TENSILE NON-AGRICULTURAL TIMBER POST AND 7 WIRE FENCE

Designed	J.CALLIS / A. LOVETT	Signed	[Signature]	Date	05/02/21
Drawn	A.MORGAN	Signed	[Signature]	Date	05/02/21
Checked	G.MICHAS	Signed	[Signature]	Date	05/02/21
Approved	K.CHILES	Signed	[Signature]	Date	05/02/21

Scale(s) 1:10 ELR & Mileage

Sheet 1 of 1

Drawing Number NR/OTK/SD/BM/101 Revision **B01**



- NOTES:
- SPECIFICATION DRAWING OR COMPONENT DEVIATIONS REQUIRE PRE INSTALLATION APPROVAL (IN WRITING) FROM THE EMPLOYER'S REPRESENTATIVE.
 - ALL FENCE COMPONENTS SHALL HAVE STATED UNDERWRITTEN MINIMUM WARRANTY AS INSTALLED OF 25 YEARS.
 - ALL FIXINGS DEPLOYED AS DETAILED. ALL FULLY TIGHT AND SHEARED.
 - CONCRETE FOUNDATION DIMENSIONS ARE MINIMUMS.
 - WELD MESH FENCE SECURED WITH CL35 STAINLESS STEEL ROLL CLIPS EVERY 210mm TOP WIRE, 420mm MIDDLE WIRE AND EVERY 350mm BOTTOM WIRE.
 - Ø3.15mm HIGH TENSILE GALVANISED LINE WIRE.
 - ALL POSTS TYPES GALVANISED ROUNDED STEEL ANGLES WITH GALVANISED BASE PLATES OR SHS ALTERNATIVE AS SHOWN (WITH 40mm x 25mm x 5mm WELDED LUGS C/W 10mmØ HOLE FOR FIXINGS AT LINE WIRE AND STRUT POSITIONS FOR STRAINERS ONLY)
 - WELD MESH 50mm x 50mm x 3mm. ALL METALWORK HOT DIP GALVANISED.
 - CONCRETE OF 20 KG PER BAG. -INTERMEDIATE STRAINER 3 BAGS FOR SQUARE FOUNDATION AND 2.5 BAGS FOR CIRCULAR, -STRUT 3 BAGS, -STRAINER 6 BAGS.
 - NO ROUND ALTERNATIVE ALLOWED FOR RSA OR SHS STRAINER FOUNDATION.
 - NUMBER, PLACEMENT AND LAYOUT OF STRAINING POSTS SHOULD BE IN ACCORDANCE WITH BS1722-10.
 - SPECIFICATION FOR ANTI-INTRUDER FENCES IN WELDED MESH ACCORDANCE TO BS1722-10.
 - TO BE READ AND USED ALONGSIDE MANDATORY COMPONENT SPECIFICATION DOCUMENT (NR/OTK/SD/BM/105/SPEC) AND BOUNDARY MEASURE MANAGEMENT MANUAL (NR/L2/OTK/5/100). THESE DOCUMENTS OUTLINE REQUIREMENTS AND EXCLUSIONS FOR THE SPECIFIC FENCING TYPE.

Rev	Date	Description of Revisions	Drawn	Chkd	Appr
B01	05/02/21	ISSUE FOR APPROVAL	AM	GM	KC
A02	16/12/20	AMENDMENTS FROM REVIEW	AM	GM	KC
A01	17/07/20	ISSUE FOR APPROVAL	AM	GM	KC
P04	28/02/19	AMENDMENTS FROM REVIEW	VK		

Status

PUBLISHED



Contractor(s)

Project

BOUNDARY MEASURE STANDARD DRAWING

Drawing Title

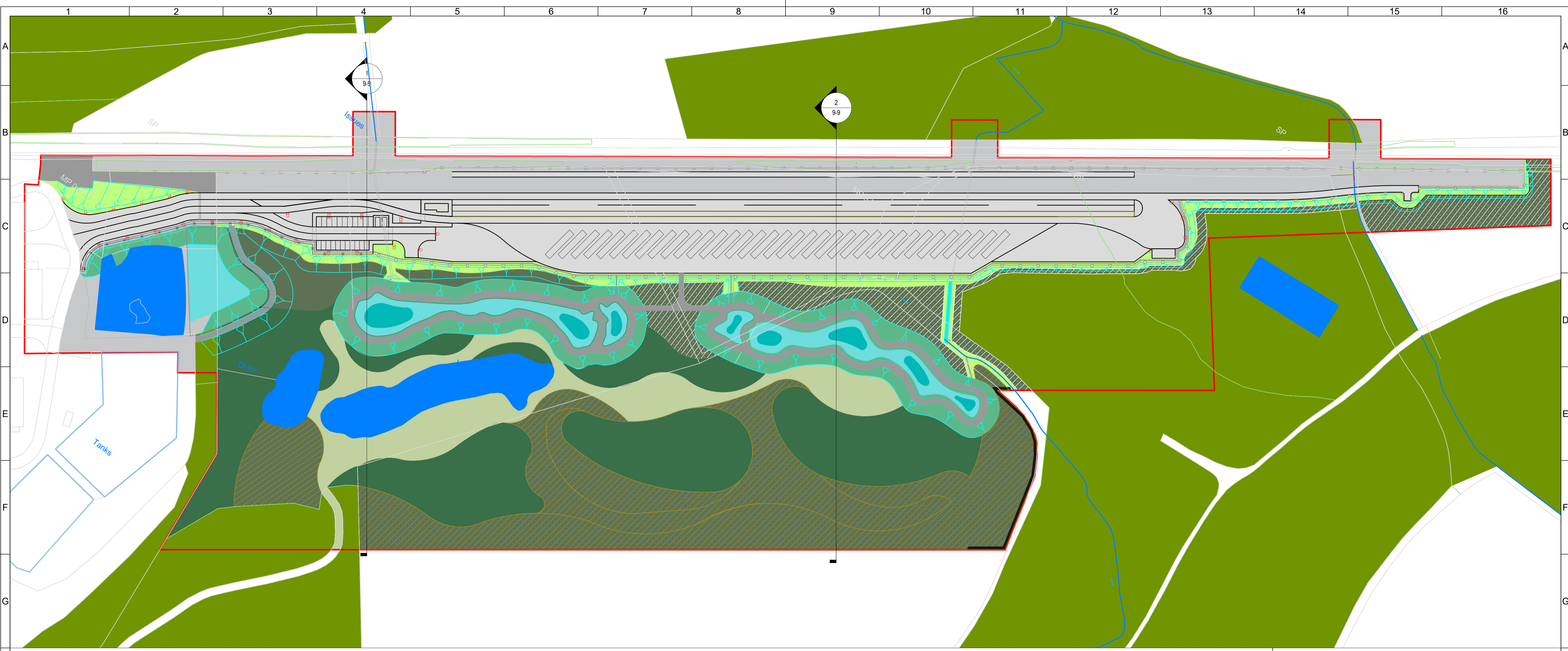
CLASS II - WELDED MESH - MESH ROLL STEEL POST FENCE

Designed	J.CALLIS / A. LOVETT	Signed	[Signature]	Date	05/02/21
Drawn	A.MORGAN	Signed	[Signature]	Date	05/02/21
Checked	G.MICHAS	Signed	[Signature]	Date	05/02/21
Approved	K.CHILES	Signed	[Signature]	Date	05/02/21

Scale(s) 1:10 ELR & Mileage

Sheet 1 of 1

Drawing Number NR/OTK/SD/BM/105 Revision B01



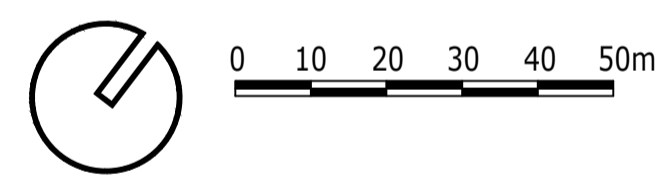
Key:

- Red Line Boundary
- Existing vegetation / hedgerows retained
- Existing watercourses
- Proposed ditch
- Existing ditch/watercourse to be removed
- Extent of Earthworks Area
- LE1.1 Arrhenatherum neutral grassland with scattered rushes and scrubs (g3c5)
- LE1.2 Other neutral grassland with scattered rushes (g3c)
- LE2.1 Woodland - other woodland mix (w1h)
- LE2.2 Woodland - other Scot's Pine woodland (w2b)
- LE2.3 Woodland - upland birchwood (w1e)
- LE2.8 Mixed scrub (h3h)
- LE3.1 Native hedgerow
- LE4.1 Reedbed
- LE4.2 Other wetland (f2f)
- Permanent water
To base of ponds

Indicative species list

- LE 1.1 Arrhenatherum neutral grassland with scattered rushes and scrubs (g3c5)**
Creeping thistle, Cock's foot, False oat grass (D), nettle, white clover, perennial ryegrass (O), harebell (R)
- LE1.2 Other neutral grassland with scattered rushes (g3c)**
Ryegrasses, Timothy, Cock's foot, Meadow fescue, White clover, Common bent, False oat grass, Yorkshire fog (A)
- LE2.1 Other woodland mixed (w1h) :**
Pedunculate oak (D), Sycamore, Elder, Ash (O), Rowan, Gorse, (A), scattered Beech and Scot's Pine (O)
- LE2.2 Other Scot's Pine woodland (w2b):**
Scots pine (D), Silver birch, Rowan (F), Juniper (O), Elder (O).
- LE2.3 Upland birchwoods (w1e)**
Birch spp.(D), Rowan, Juniper (F), Aspen (R)
- LE2.8 Mixed scrub (h3h)**
Elder, Broom (D), Gorse, Bramble (F), Silver birch, Raspberry, Ragwort (F), Rowan, Sycamore (O)
- LE3.1 Native hedgerow**
Elder, Broom (D), Gorse, Raspberry, Rowan
- LE4.1 Other Wetland (f2f)**
Rush spp.(D), Great reedmace (A), Thistle spp. (A), Willowherb spp., Grey willow (F), Broom (F), Carex spp.
- LE4.2 Other standing water (r1g)**
Reedbed

- Notes**
1. Do not scale from this drawing.
 2. This drawing is to be read in conjunction with all other relevant drawings and specifications.
 3. Any discrepancies, errors or omissions to be brought to the attention of the designer.



Notes

1. This drawing is not to scale.
2. This drawing is to be read in conjunction with all other relevant drawings and specifications.
3. Any discrepancies, errors or omissions to be brought to the attention of the designer.

P4	24/04/2025	AREA OF SCOTS PINE WOODLAND INCREASED	SL	KT	RW
P3	05/12/2024	UPDATES AFTER COMMENTS	SL	KT	IL
P2	11/10/2024	DRAFT FOR COMMENTS	SL	KT	IL
P1	20/09/2024	DRAFT FOR COMMENTS	KT	IL	RW
Rev	Date	Status	Drwn	Chkd	Appd

Client
WEST FRASER MILLS

Project
WEST FRASER RAIL SIDINGS

Title
LANDSCAPE MITIGATION PLAN

Status
ISSUED FOR COMMENTS

Scale
1:500 @ A1

Drawing number
9-8 Landscape Mitigation Plan



SSE Drawing number 1620016155	Revision P4
---	-----------------------