

Agenda Item	5.1
Report No	PLS/16/26

## HIGHLAND COUNCIL

**Committee:** South Planning Applications Committee  
**Date:** 06 February 2025  
**Report Title:** 24/02815/FUL: Ms Yvonne Birrell  
Delmore Bungalow, Inverness, IV3 8RG  
**Report By:** Area Planning Manager – South

### Purpose/Executive Summary

**Description:** Erection of house  
**Ward:** 13 – Inverness West  
**Development category:** Local

**Reason referred to Committee:** More than 5 Objections / technical Objection

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

### Recommendation

Members are asked to agree the recommendation to **GRANT** the application as set out in section 11 of the report

## 1. PROPOSED DEVELOPMENT

- 1.1 The proposal is for the erection of a replacement house on a site at Delmore, west of Inverness. The previous house on the site, Delmore Bungalow, was erected circa 1958 and was located on the northwest boundary of the site, close to the steep embankment adjoining the railway line. Delmore Bungalow was demolished after suffering structural damage following a landslip in 2011 which affected the stability of the embankment and required Network Rail to take measures to stabilise it.
- 1.2 Planning permission in principle was granted for a replacement house in 2019 (19/03116/PIP) on the same site but set back from the embankment, around 20m from the original house. This permission has now expired.
- 1.3 The site is accessed via the U2381 public road which leads from the Scorguie area of Inverness. Access to the site itself utilises a small private track taken from the end of the U2381. This access is not considered to be suitable for construction traffic therefore a temporary construction access track across the field from the U2381 to the application site forms part of this application.
- 1.3 Pre-Application Consultation: No pre-application advice
- 1.4 Supporting Information: Drainage Statement
- 1.5 Variations: Details of temporary construction access provided

## 2. SITE DESCRIPTION

- 2.1 The site slopes upwards from north to south and is accessed from the west by a private track leading from the U2381 public road. The site sits above the railway line and the A862 Inverness to Beauly road, which lie to just north of the application site. It is edged by trees to the north and east and is more open to the south. The remains of Delmore Bungalow and associated hardstanding are still clearly visible on site. The site is surrounded by agricultural land.

## 3. PLANNING HISTORY

3.1	10.07.2024	23/01222/FUL Erection of house	Planning Application Withdrawn
3.2	04.11.2019	19/03116/PIP Erection of house	Planning Permission Granted
3.3	27.02.2019	19/00036/PIP Erection of replacement house	Planning Application Withdrawn
3.4	03.07.2019	19/01802/PIP Erection of house	Planning Application Withdrawn

3.5 02.02.2018 17/05809/FUL Slope stability works by Network Rail Planning Permission Granted

#### 4. PUBLIC PARTICIPATION

4.1 Advertised: Unknown Neighbour

Date Advertised: 19.07.2024

Representation deadline: 02.08.2024

Timeous representations: 11 representations from 11 households

Late representations: 0

4.2 Material considerations raised are summarised as follows:

##### Principal

- a) Original planning permission in principle had expired before the new application was submitted therefore the principle of the proposal needs to be reassessed

##### Policy

- b) Proposal does not comply with Rural Housing Supplementary Guidance which states that any replacement house should be within the curtilage of the existing house and should not result in an excessive increase in footprint of the original house. Current proposal has floorspace over 4 times the replaced property

##### Design

- c) Replacement house and should therefore fit the curtilage of the previous house. The proposed house is significantly larger - 2 storeys compared with the original single storey property and 4 bedrooms instead of 2 - will have a negative visual impact.
- d) Will not be consistent with neighbouring houses in terms of scale and design.

##### Access

- e) Private track not in a condition to support construction traffic.
- f) Note the Transport Planning's response that the track must be fit for purpose and have necessary maintenance agreements in place.
- g) Additional traffic will pose a risk to vulnerable road users, including cyclists, walkers, runners and horse riders.
- h) Impact on the single-track public road (U2381) especially from construction traffic. Road is very narrow in places with a steep embankment and very few passing places - several vehicles have gone off the road in the past. Roads have previously Objected to any new development along this road.
- i) If granted, a condition to ensure reinstatement of any temporary access track should be included to protect the agricultural land.
- j) Applicant has no right of access from the northwest over the railway line, and the Network Rail has Objected to its use therefore all traffic will have to come via the private access track from the southwest. Serious concerns about the

impact of the development on the track, which is maintained by the owners of Bruichnain House, Oakbank, Tigh na Feidh and Delmore Bungalow. The track will not sustain an increased level of traffic without major upgrading. There are virtually no passing places and no scope to provide new passing places and there is a weight limit of 4 tonnes due to a fragile culvert. Owners and other users do not support a maintenance agreement as suggested by Transport Planning.

- k) All refuse would need to be taken to the junction with the public road which would be a significant burden for future occupants.
- l) Proposed temporary construction access taken from a straight stretch of road, but with blind bends/braes in each direction.

#### Drainage

- m) There have been several incidents in the past 20 years where surface water has come down the private road to the railway line. Would be concerned about proposed changes to the site which may reduce natural drainage.

#### Natural heritage

- n) Potential impact on badgers, red squirrels, pine martens, bats etc.

4.3 All letters of representation are available for inspection via the Council's eplanning portal which can be accessed through the internet [www.wam.highland.gov.uk/wam](http://www.wam.highland.gov.uk/wam).

## 5. CONSULTATIONS

5.1 **Transport Planning Team** objected to a number of recent planning applications accessed from the U2381 due to the substandard geometry of the road. The road is single track with a number of passing places, however there are substantial issues with regard to forward visibility, narrow road width and lack of passing places along its length. It has developed over time from a farm track and therefore does not meet modern standards in terms of construction and geometry. It is also a popular pedestrian and cycle route. The road is not suitable for any additional traffic, especially construction traffic and therefore the Transport Planning Team will not support any further development here.

In addition, the private track to the house is also unfit for additional traffic. It is steep, extremely narrow with virtually no passing places with grass growing through the surface. Council guidance does permit up to four houses off a private track, however the track must be fit for purpose and the applicant must provide a maintenance agreement which is supported by all those who live on the track or who have a right of access over it. However, given the extremely poor condition of the track, combined with its steepness, narrowness and lack of passing places, the Transport Planning Team would not support any additional dwellings to be built off this track.

The Transport Planning Team note that it is proposed to create a temporary construction access from the U2381. No objection to the principle of a temporary track subject to the applicant demonstrating that the appropriate visibility splays from the track onto the U2381 can be provided and the access point designed as per Council standards.

Therefore, as the U2381 and the private track to this proposed dwelling are unsuitable for additional traffic, the Transport Planning Team object to this application.

- 5.2 **Network Rail:** No objection, however due to its close proximity to the operational railway line, advisory notes are recommended to ensure Bruichnain Level Crossing is not used to access the development site and construction works are carried out in a safe manner which does not disturb the operation of the railway line.

## 6. DEVELOPMENT PLAN POLICY

The following policies are relevant to the assessment of the application

### 6.1 National Planning Framework 4 (2023) (NPF4)

Policy 1 - Tackling the Climate and Nature Crises  
Policy 2 - Climate Mitigation and Adaptation  
Policy 3 - Biodiversity  
Policy 6 - Forestry, Woodland and Trees  
Policy 9 - Brownfield, Vacant and Derelict Land and Empty Buildings  
Policy 13 - Sustainable Transport  
Policy 14 - Design Quality and Place  
Policy 17 - Rural Homes  
Policy 18 - Infrastructure First  
Policy 22 - Flood Risk and Water Management

### 6.2 Highland Wide Local Development Plan 2012 (HwLDP)

28 - Sustainable Design  
29 - Design Quality and Place-making  
30 - Physical Constraints  
31 - Developer Contributions  
35 - Housing in the Countryside (Hinterland areas)  
51 - Trees and Development  
56 - Travel  
57 - Natural, Built and Cultural Heritage  
58 - Protected Species  
64 - Flood Risk  
65 - Waste Water Treatment  
66 - Surface Water Drainage

### 6.3 Inner Moray Firth Local Development Plan 2 (2024) (IMFLDP2)

No specific policies apply.

### 6.4 Highland Council Supplementary Planning Policy Guidance

Access to Single Houses and Small Housing Developments (May 2011)  
Developer Contributions (March 2018)  
Flood Risk and Drainage Impact Assessment (Jan 2013)  
Highland's Statutorily Protected Species (March 2013)  
Housing in the Countryside and Siting and Design (March 2013)  
Sustainable Design Guide (Jan 2013)  
Trees, Woodlands and Development (Jan 2013)

## **7. OTHER MATERIAL POLICY CONSIDERATIONS**

7.1 None

## **8. PLANNING APPRAISAL**

8.1 Section 25 of the Town and Country Planning (Scotland) Act 1997 requires planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.

### **Determining Issues**

8.2 This means that the application requires to be assessed against all policies of the Development Plan relevant to the application, all national and local policy guidance and all other material considerations relevant to the application.

### **Planning Considerations**

8.3 The key considerations in this case are:

- a) compliance with the development plan and other planning policy;
- b) siting and design;
- c) access and infrastructure;
- d) any other material considerations.

### **Development plan/other planning policy**

8.4 The application site sits just outside the Settlement Development Area of Inverness, therefore the relevant policy within NPF4 is Policy 17 (Rural Homes). This seeks to encourage, promote and facilitate the delivery of more high quality, affordable and sustainable rural homes in the right locations. For the purposes of this policy the application site is within the area defined as Accessible Rural, where development is restricted, unless it meets one of the 8 development types set out in NPF4. One such development type is for a proposal which “reinstates a former dwelling house or is a one-for-one replacement of an existing permanent house”.

8.5 HwLDP Policy 35 (Housing in the Countryside (Hinterland areas)) is also supportive of development proposals which replace existing houses, and the associated Supplementary Guidance (Rural Housing) (2021) document further states that “where a habitable house is damaged or destroyed beyond repair by an external factor (i.e. fire or flood), its replacement will generally be supported”. Therefore, subject to design criteria set out in the Supplementary Guidance and other relevant policies and technical matters it is considered that the proposal can be supported in principle under the development plan.

### **Siting and Design**

8.6 The Rural Housing Supplementary Guidance sets out some general design criteria for replacement houses: The replacement house should be of a form and scale relative to its original; the use of existing site features, boundaries and access points

should be used where appropriate; and the new house should be sited to maximise solar gain, protection from the prevalent weather and avoid areas of overshadowing.

- 8.7 The Supplementary Guidance further states that the footprint of the replacement house should not result in an excessive increase to that of the original house. It is noted that a previous application in 2023 (23/01222/FUL) for a much larger house on the site was withdrawn by the applicant following discussions, as it was not considered to comply with the above design criteria due to its size and scale. This proposal is for a significantly reduced footprint, which while still considerably larger than the small house it replaces, is by no means excessive in scale in comparison to the other houses in the area or to an average, modern home. There was also a garage and outbuilding on the site pre-demolition which added to the original built footprint on the site. Taken together, the proposed footprint is not significantly greater than that of the original buildings. There are no close neighbouring properties and no overlooking or privacy concerns.
- 8.8 The proposed house design is for a contemporary mono-pitched, 2 storey, split level house which makes good use of the sloping site and reduces visual impact. The upper floor incorporates large windows and a glazed balcony with views to the Firth, and the walls are clad in blackened timber which will blend well into the surrounding landscape. The ground floor also utilises traditional materials with white render and natural stone walls. Overall, the proposed design is considered to make a positive contribution to the area and is acceptable.
- 8.9 The Rural Housing Supplementary Guidance states that any replacement house should sit within the curtilage of the original house and should only be relocated within the site where benefits to the landscape setting, improved biodiversity or environmental constraints are achieved. In this instance, it is considered that the proposed relocation, 20m to the southeast of the original house, is acceptable given the history of land instability and the embankment formed as part of Network Rail works to stabilise the works and protect the railway. The proposed house is still within the curtilage of the demolished house, utilises the same access point and does not impact upon the amenity of any neighbouring property. It is therefore considered to comply with policy in terms of siting.

### **Access and Infrastructure**

- 8.10 The proposal is for the completed house to be accessed via a private track taken from the end of the single track U2381 public road. This private track served the former Delmore Bungalow and provides access to 3 other houses to the south of the site then continues down to the railway and two further properties. The applicant has a legal right of access over the track. As noted by Transport Planning, it is very narrow and steep with virtually no passing opportunities other than those provided by the driveways of other properties. It is in poor condition, with grass growing through its centre and tree canopies overhead. Council guidance does allow for up to four houses to be accessed from a private track, however, while the Council does not have remit over private roads, given the poor state of the track, Transport Planning do not recommend further use of the private track.
- 8.11 While the previous permission in principle (19/03116/PIP) granted in 2019 for the replacement house did allow use of the U2381 public road and the private track for

access and construction traffic, Transport Planning has consistently objected to all more recent applications for new developments off the U2381 due to its substandard geometry, substantial issues with regard to forward visibility, narrow road width and lack of passing places along its length. It notes that the U2381 has developed over time from a farm track and therefore does not meet modern standards in terms of construction and geometry. It is also a popular pedestrian and cycle route. Transport Planning conclude that the road is not suitable for any additional traffic, especially construction traffic, and do not support any further development which utilises it.

- 8.12 The application received 11 objections - a significant number given the small local population. All of these refer to the poor state of the public and private access to the site, and also note the recent planning refusals on technical matters relating to the road.
- 8.13 The Planning Officer fully acknowledges and concurs with the road related concerns raised by Transport Planning and local residents, and confirms that no new development accessed via the U2381 public road will be supported without it being significantly improved to the satisfaction of the Roads Authority. However, while the technical issues and objection by Transport Planning would have resulted in a clear reason for refusal for this application were it for a brand-new house on this site, the situation is less clear-cut than this. This proposal is not for a new house but is for the replacement of a house which had to be demolished following a natural event which was wholly outwith the applicant's control. While it is acknowledged that a considerable amount of time has passed since the house was demolished, in planning terms the use of the site remains residential and therefore the replacement house would not be seen to alter the character of the site or intensify the use of the U2381 or the private track, as it would simply be replacing one house with another.
- 8.14 The concerns regarding the suitability of the private track to accommodate construction vehicles were noted and a temporary construction access will be formed across the field to the south of the site from the U2381. It is noted that due to the roadside trees visibility will be substandard, which while acceptable for a temporary construction access track (with appropriate warning signage), would not be acceptable for a permanent access to the house. Therefore temporary construction (e.g. matting) should be considered and a planning condition will ensure that the track is removed and the land returned to its pre-development state post completion. On balance, under these particular circumstances, and taking account of the planning history of the site, it is considered that the proposal can be supported despite the technical objection by the Roads Authority. It should, however, be made clear that this is strictly due to the fact that the proposal is for the direct replacement of a house on the site and does not alter the fact that the U2381 is not considered suitable for any additional future development in its present state.
- 8.15 A drainage strategy setting out proposals for managing both foul and surface water within the site, has been provided in support of the application. On-site ground conditions are known to be unsuitable for soakaway use, therefore it is proposed to direct both foul and surface water into a nearby watercourse following appropriate attenuation/treatment. This watercourse is located to the east of the new development and away from neighbouring properties.

## **Other material considerations**

- 8.16 The planning history of a site is a material consideration in the assessment of any planning application. This site had planning permission in principle in 2019 (19/03116/PIP) for the erection of a replacement house, however due to delays caused by the COVID 19 pandemic and the personal circumstances of the applicant, an MSC application was not submitted in time and the permission lapsed. It therefore needs to be considered afresh against current policies and technical advice, however the fact that it did have a recent permission in principle is a factor in its favour.

## **Non-material considerations**

- 8.12 Future maintenance of the private access track is a private legal matter for its owner and users of the track and is not a material planning consideration.

## **Matters to be secured by Legal Agreement / Upfront Payment**

- 8.13 No developer contributions are required.

## **9. CONCLUSION**

- 9.1 The assessment of this application is not clear-cut. On one side there is a policy justification for the replacement of a house which had to be demolished through no fault of the applicant. On the other side are valid technical objections from Transport Planning, echoed by public representations, about the poor state of the road and its capacity for additional traffic.
- 9.2 Ultimately, it is not considered reasonable to refuse to allow the replacement of a house which needed to be demolished under such circumstances, particularly when it has recently been granted planning permission in principle. The applicant has taken cognisance of concerns about the scale of the house and a previous application for a much larger house was withdrawn and replaced with this application for a smaller building of good design and utilising good quality, traditional materials.
- 9.3 Transport Planning has taken a consistent approach to development proposals accessed via the U2381 and this is welcomed. It sends a strong message to would be developers that proposals for new developments utilising this road will not be supported. Officer support for this particular application should not be seen to alter this. It must be stated very clearly that the recommendation to grant this application, against the advice of Transport Planning, is made only due to the fact that this is not considered to be a new development, but is the replacement of a house, under specific circumstances, and the impact is therefore not considered to be significant. For the reasons set out in this report, any future proposal for new developments which would increase traffic on this road would be very unlikely to receive a positive recommendation.
- 9.4 All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## 10. IMPLICATIONS

- 10.1 Resource: Not applicable
- 10.2 Legal: Not applicable
- 10.3 Community (Equality, Poverty and Rural): Not applicable
- 10.4 Climate Change/Carbon Clever: Not applicable
- 10.5 Risk: Not applicable
- 10.6 Gaelic: Not applicable

## 11. RECOMMENDATION

<b>Action required before decision issued</b>	N
Notification to Scottish Ministers	N
Conclusion of Section 75 Obligation	N
Revocation of previous permission	N

**Subject to the above actions**, it is recommended to **GRANT** the application subject to the following conditions and reasons

1. The development to which this planning permission relates must commence within THREE YEARS of the date of this decision notice. If development has not commenced within this period, then this planning permission shall lapse.

**Reason:** In accordance with Section 58 of the Town and Country Planning (Scotland) Act 1997 (as amended).

2. No development shall commence until a Construction Traffic Management Plan (CTMP) has been submitted to, and approved in writing by, the Planning Authority. As a minimum, the CTMP shall include:
  - Predicted volume and type of construction traffic
  - Locations for loading and unloading for materials, plant and machinery
  - Proposals for consultation with local residents
  - Proposals for appropriate warning signage
  - Construction traffic hours of operation
  - Measures to ensure the safety of the public
  - Measures to protect the public road from mud and debris
  - Pre and post construction road condition surveys carried out using GPS enabled dashcam footage, filmed in the same direction with confirmation that any extraordinary wear and tear will be repaired by the developer to the satisfaction of the Council

The approved Traffic Management Plan shall be implemented by the developer prior to development commencing and shall remain in place until the development is complete, all to the satisfaction of the Planning Authority in consultation with the Roads Authority.

**Reason:** In the interests of road safety

3. No development shall commence until full details of the temporary construction access track shall have been submitted to and approved in writing by the Planning Authority in consultation with the Roads Authority. Thereafter the track shall be constructed in accordance with the approved details and shall be fully removed from the site within 3 months of completion of the house hereby approved, and the land made good. For the avoidance of doubt, the temporary construction track shall be formed, where possible, using a temporary overlay matting.

**Reason:** In the interests of road safety and to reflect the temporary nature of the track

4. No development shall commence until a detailed Landscape Plan and Maintenance Programme shall have been submitted to, and received the approval in writing of, the Planning Authority. The approved Landscape Plan shall be implemented in full during the first planting season following commencement of development, with maintenance thereafter being carried out in accordance with the Maintenance Programme. For the avoidance of doubt, any trees or plants which within a period of five years from the completion of the development die, for whatever reason are removed or damaged shall be replaced in the next planting season with others of the same size and species.

**Reason:** In order to ensure that the approved landscaping works are properly undertaken on site.

## **REASON FOR DECISION**

All relevant matters have been taken into account when appraising this application. It is considered that the proposal accords with the principles and policies contained within the Development Plan and is acceptable in terms of all other applicable material considerations.

## **INFORMATIVES**

### **Initiation and Completion Notices**

The Town and Country Planning (Scotland) Act 1997 (as amended) requires all developers to submit notices to the Planning Authority prior to, and upon completion of, development. These are in addition to any other similar requirements (such as Building Warrant completion notices) and failure to comply represents a breach of planning control and may result in formal enforcement action.

1. The developer must submit a Notice of Initiation of Development in accordance with Section 27A of the Act to the Planning Authority prior to work commencing on site.
2. On completion of the development, the developer must submit a Notice of Completion in accordance with Section 27B of the Act to the Planning Authority.

Copies of the notices referred to are attached to this decision notice for your convenience.

### **Flood Risk**

It is important to note that the granting of planning permission does not imply there is an unconditional absence of flood risk relating to (or emanating from) the application site. As per Scottish Planning Policy (paragraph 259), planning permission does not remove the liability position of developers or owners in relation to flood risk.

### **Scottish Water**

You are advised that a supply and connection to Scottish Water infrastructure is dependent on sufficient spare capacity at the time of the application for connection to Scottish Water. The granting of planning permission does not guarantee a connection. Any enquiries with regards to sewerage connection and/or water supply should be directed to Scottish Water on 0845 601 8855.

### **Septic Tanks and Soakaways**

Where a private foul drainage solution is proposed, you will require separate consent from the Scottish Environment Protection Agency (SEPA). Planning permission does not guarantee that approval will be given by SEPA and as such you are advised to contact them direct to discuss the matter (01349 862021).

### **Local Roads Authority Consent**

In addition to planning permission, you will require one or more separate consents (such as road construction consent, dropped kerb consent, a road openings permit, occupation of the road permit etc.) from the Area Roads Team prior to work commencing. These consents may require additional work and/or introduce additional specifications and you are therefore advised to contact your local Area Roads office for further guidance at the earliest opportunity.

Failure to comply with access, parking and drainage infrastructure requirements may endanger road users, affect the safety and free-flow of traffic and is likely to result in enforcement action being taken against you under both the Town and Country Planning (Scotland) Act 1997 and the Roads (Scotland) Act 1984.

Further information on the Council's roads standards can be found at: <http://www.highland.gov.uk/yourenvironment/roadsandtransport>

Application forms and guidance notes for access-related consents can be downloaded from:

[http://www.highland.gov.uk/info/20005/roads\\_and\\_pavements/101/permits\\_for\\_working\\_on\\_public\\_roads/2](http://www.highland.gov.uk/info/20005/roads_and_pavements/101/permits_for_working_on_public_roads/2)

### **Mud and Debris on Road**

Please note that it is an offence under Section 95 of the Roads (Scotland) Act 1984 to allow mud or any other material to be deposited, and thereafter remain, on a public road from any vehicle or development site. You must, therefore, put in place a strategy for dealing with any material deposited on the public road network and maintain this until development is complete.

### **Construction Hours and Noise-Generating Activities**

You are advised that construction work associated with the approved development (incl. the loading/unloading of delivery vehicles, plant or other machinery), for which noise is audible at the boundary of the application site, should not normally take place outwith the hours of 08:00 and 19:00 Monday to Friday, 08:00 and 13:00 on Saturdays or at any time on a Sunday or Bank Holiday in Scotland, as prescribed in Schedule 1 of the Banking and Financial Dealings Act 1971 (as amended).

Work falling outwith these hours which gives rise to amenity concerns, or noise at any time which exceeds acceptable levels, may result in the service of a notice under Section 60 of the Control of Pollution Act 1974 (as amended). Breaching a Section 60 notice constitutes an offence and is likely to result in court action.

If you wish formal consent to work at specific times or on specific days, you may apply to the Council's Environmental Health Officer under Section 61 of the 1974 Act. Any such application should be submitted after you have obtained your Building Warrant, if required, and will be considered on its merits. Any decision taken will reflect the nature of the development, the site's location and the proximity of noise sensitive premises. Please contact [env.health@highland.gov.uk](mailto:env.health@highland.gov.uk) for more information.

### **Protected Species – Halting of Work**

You are advised that work on site must stop immediately, and NatureScot must be contacted, if evidence of any protected species or nesting/breeding sites, not previously detected during the course of the application and provided for in this permission, are found on site. For the avoidance of doubt, it is an offence to deliberately or recklessly kill, injure or disturb protected species or to damage or destroy the breeding site of a protected species. These sites are protected even if the animal is not there at the time of discovery. Further information regarding protected species and developer responsibilities is available from NatureScot: <https://www.nature.scot/professional-advice/protected-areas-and-species/protected-species>

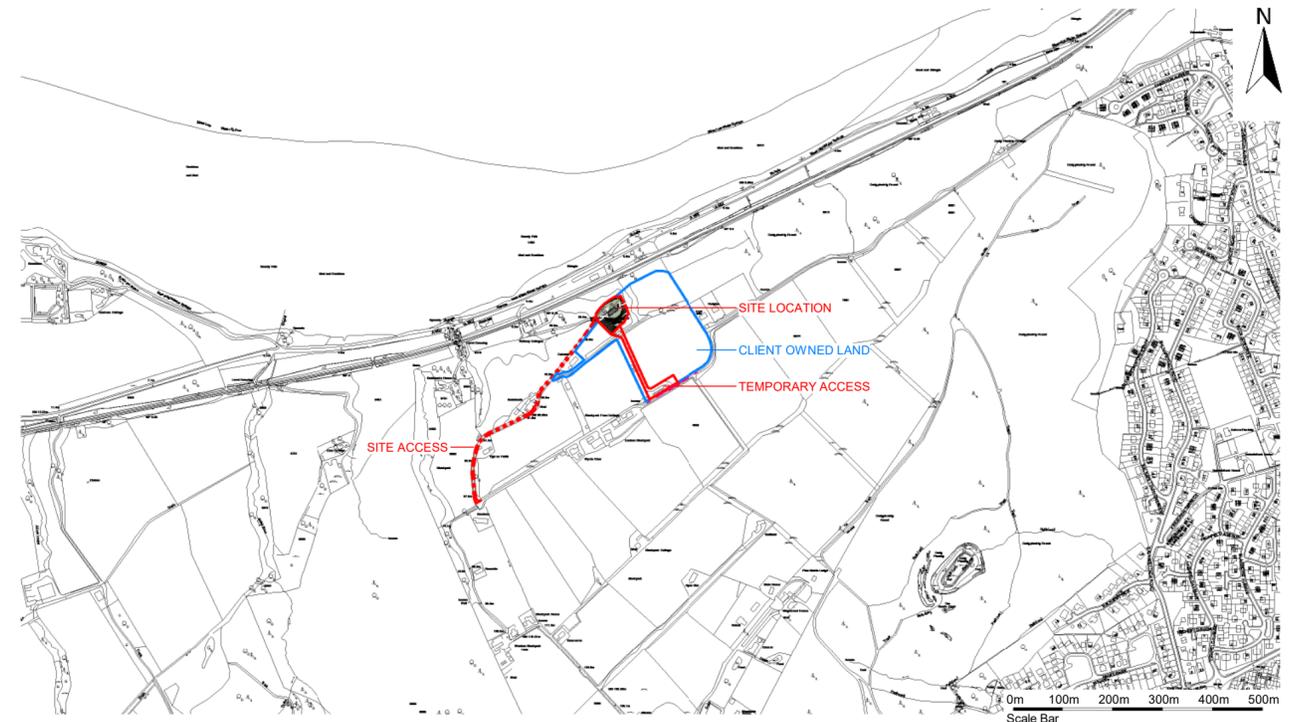
Signature: David Mudie  
Designation: Area Planning Manager – South  
Author: Christine Macleod  
Background Papers: Documents referred to in report and in case file.

Relevant Plans: Plan 1 - 2023-10-MRH-202 REV C - Location/Site Layout  
Plan 2 - 2023-10-MRH-200 - Elevation Plan  
Plan 3 - 2023-10-MRH-201 - Floor Plan  
Plan 4 - 2023-10-MRH-204 – Landscaping Plan  
Plan 5 - CTCH-J5066-001 – Drainage Plan

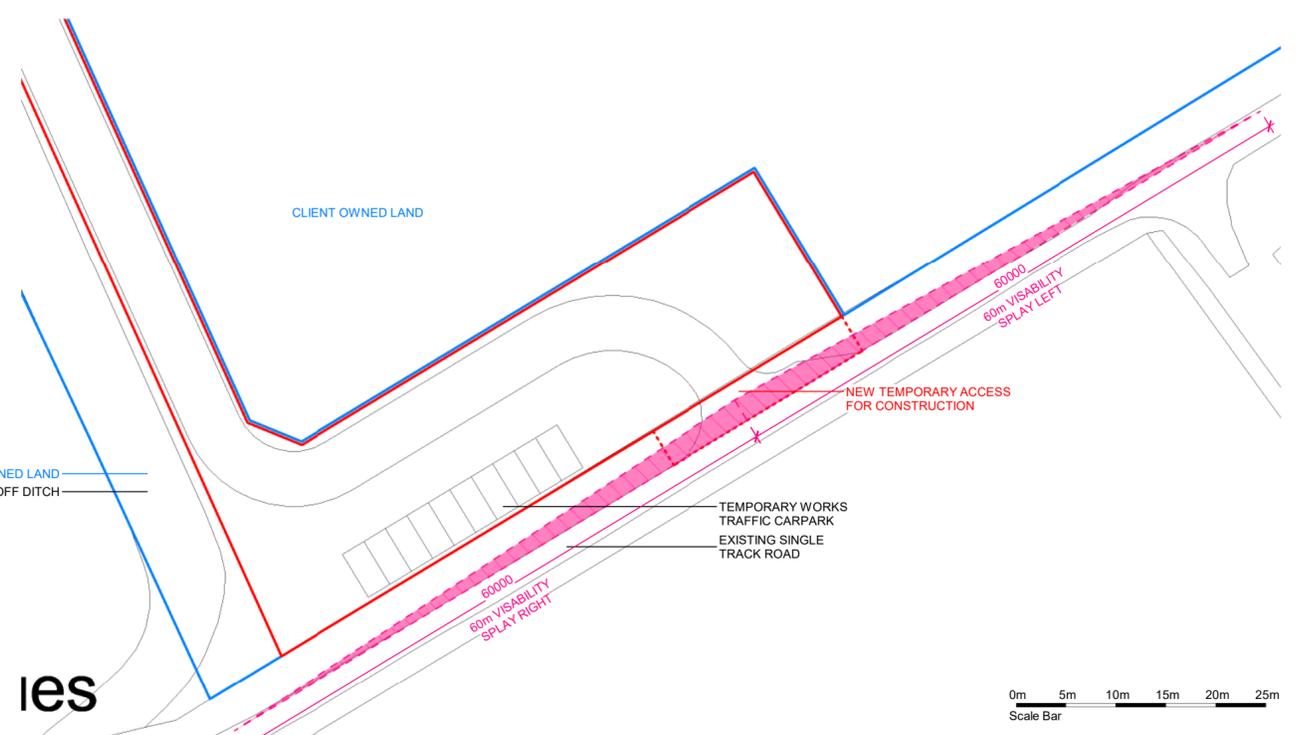
# PROPOSED - PLOT AT DELMORE BUNGALOW, BLACKPARK



**1** Block Plan  
1 : 500  
SITE AREA = 3082m<sup>2</sup>



**2** Location Plan  
1 : 10000  
REPRODUCTION BASED UPON THE ORDANANCE SURVEYS WITH THE PERMISSION OF THE CROWN. COPYRIGHT RESERVED



ies  
Scale Bar  
0m 5m 10m 15m 20m 25m



Client Name	<b>Ms Y Birrell</b>			
Project Name	Proposed Dwelling and Garage AT Delmore Bungalow, Blackpark, IV3 8RG			
Title / Description	Proposed - Location_Block Plan			
Scale	As indicated	Project No. <b>2023-10</b>		
Date	July 2024	Drawing No.		
Drawn by	KHS	<b>2023-10-MRH-202</b>		
rev.	REVa - KH	REVb - KHS	REVc - KHS	A2

THE STUDIO  
No 9  
Heights of Woodside  
Westhill  
Inverness IV2 5TH  
Tel/Fax: 01463 794410  
Mobile: 07760195141  
Email: info@mrhdesign.co.uk  
Web: www.mrhdesign.co.uk

DO NOT SCALE, IF IN DOUBT PLEASE ASK  
All setting out must be checked on site prior to commencement.  
Any discrepancy must be reported to client.

This drawing is the sole copyright of MRH Architectural Design & Planning and should not be copied or reproduced without prior consent.

# PROPOSED - PLOT AT DELMORE BUNGALOW, BLACKPARK



**1** North West Elevation  
1 : 100



**2** South West Elevation  
1 : 100



**3** South East Elevation  
1 : 100



**4** North East Elevation  
1 : 100



**5** 3D Artistic Impression

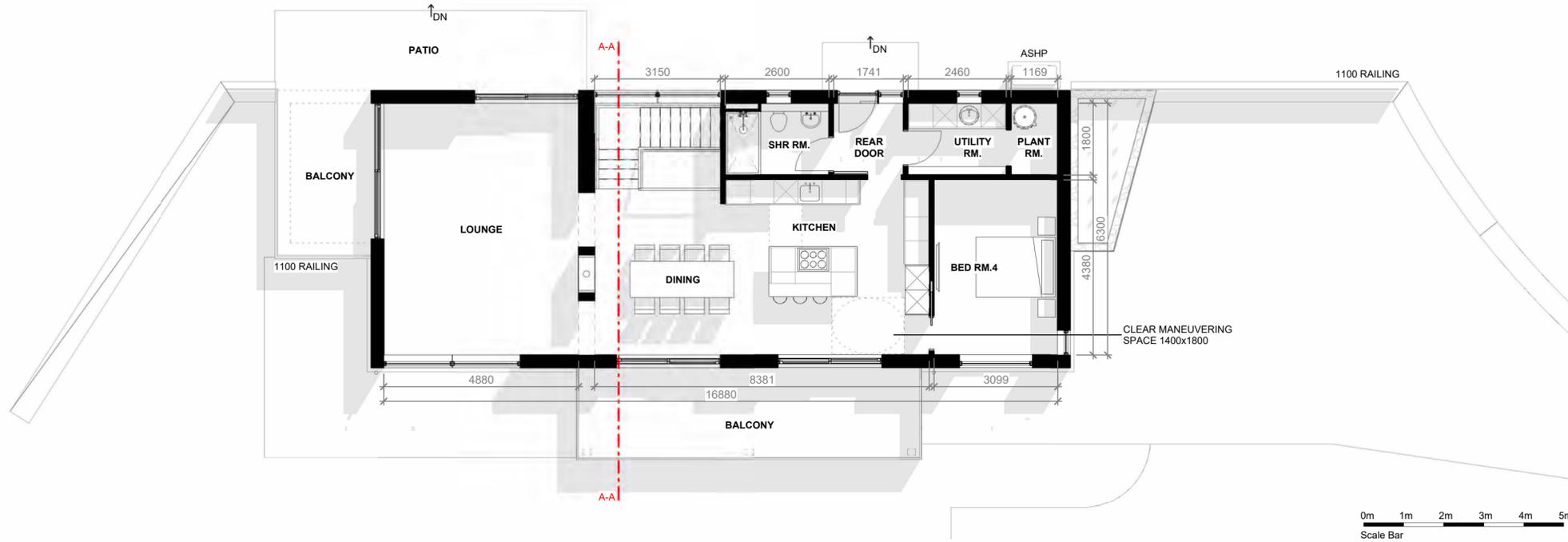
MATERIALS	
WALLS -	SCRATCH RENDER (OFF WHITE), VERTICAL TREATED TIMBER (BLACK), STACKED STONE (DARK GREY)
ROOF -	MONO PITCH, STANDING SEAM METAL (DARK GREY)
DOORS / WINDOWS -	ALICLAD (DARK GREY)



Client Name	<b>Ms Y Birrell</b>
Project Name	Proposed Dwelling and Garage AT Delmore Bungalow, Blackpark, IV3 8RG
Title / Description	Proposed - Elevations

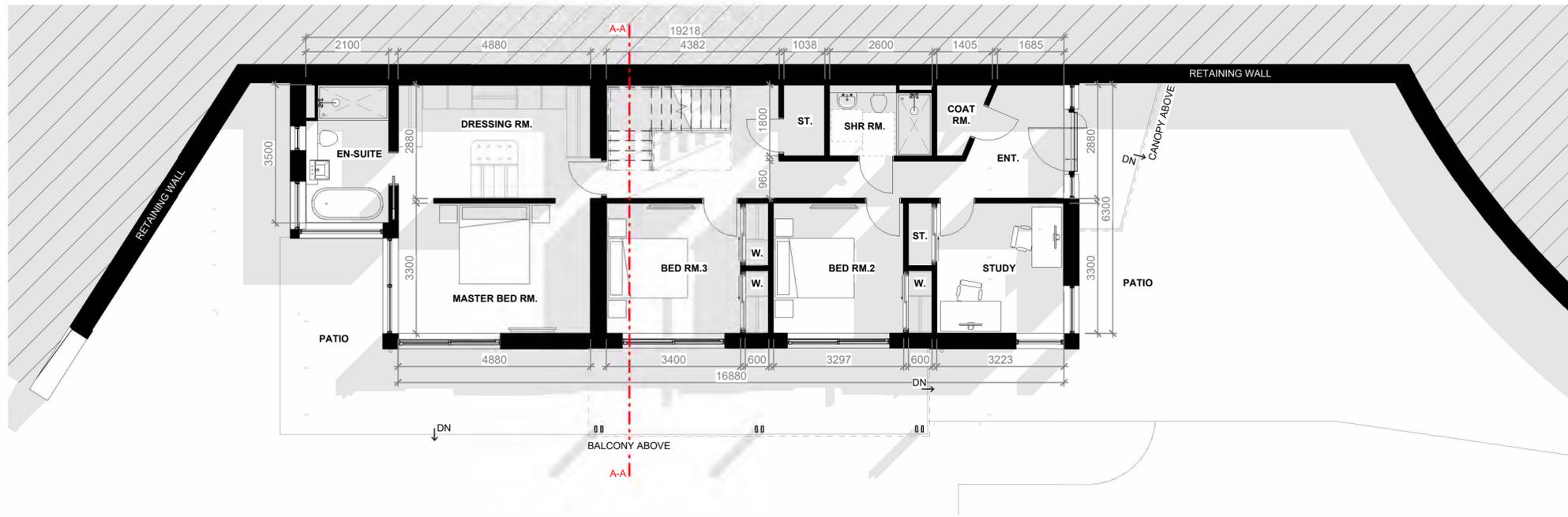
Scale	1 : 100	Project No.	<b>2023-10</b>
Date	May 2023	Drawing No.	<b>2023-10-MRH-200</b>
Drawn by	KHS		
DO NOT SCALE, IF IN DOUBT PLEASE ASK	rev.	REVa - KH	REVb - KHS
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# PROPOSED - PLOT AT DELMORE BUNGALOW, BLACKPARK



**1** Ground Floor Plan  
1 : 100

GROUND FLOOR AREA = 106m<sup>2</sup>  
LOWER FLOOR AREA = 114m<sup>2</sup>  
TOTAL FLOOR AREA = 220m<sup>2</sup>



**0** Lower Floor  
1 : 100

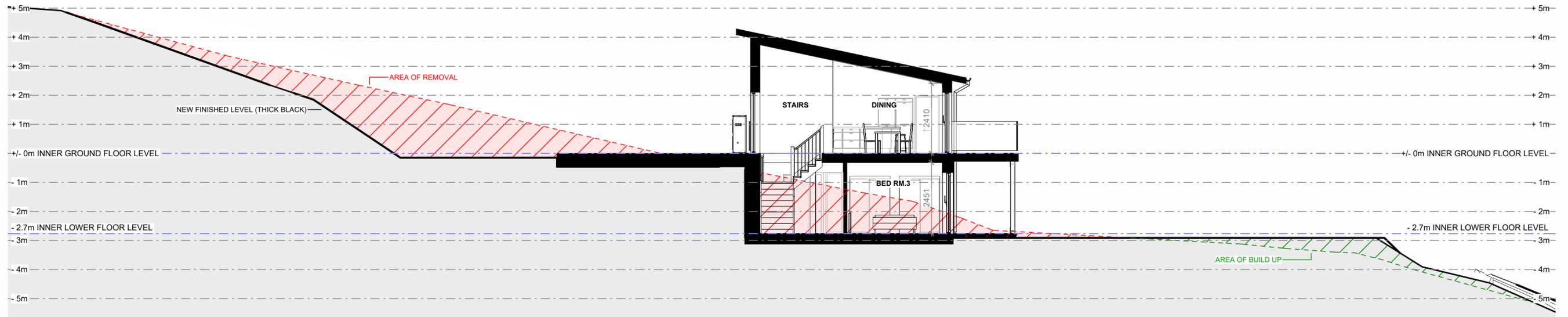


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DO NOT SCALE, IF IN DOUBT PLEASE ASK  
All setting out must be checked on site prior to commencement.  
Any discrepancy must be reported to client.

Client Name		Ms Y Birrell	
Project Name		Proposed Dwelling and Garage AT Delmore Bungalow, Blackpark, IV3 8RG	
Title / Description		Proposed - Floorplans	
Scale	1 : 100	Project No.	2023-10
Date	May 2023	Drawing No.	2023-10-MRH-201
Drawn by	KHS		
rev.	REVa - KH	REVb - KHS	-
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# PROPOSED - PLOT AT DELMORE BUNGALOW, BLACKPARK



**1** Site Section A-A  
1 : 100



**2** Landscaping Plan  
1 : 500



Client Name		
<b>Ms Y Birrell</b>		
Project Name		
Proposed Dwelling and Garage AT Delmore Bungalow, Blackpark, IV3 8RG		
Title / Description		
Proposed - Landscape_Site Section		
Scale	As indicated	Project No. 2023-10
Date	May 2023	Drawing No.
Drawn by	KHS	2023-10-MRH-204
rev.	REVa - KH	REVb - KHS
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- NOTES:-**
- Drainage to be constructed, installed & tested in accordance with the recommendations in BS EN 752:2017, BS EN 1610:2015, SEPA Regulatory Method WAT-RM-03 and SEPA GBR10.
  - A minimum of 600mm cover to be provided to all pipework within garden/landscaped areas; a minimum of 1200mm below any parking area and a minimum of 1200mm below any road. Concrete side protection to be provided where minimum cover below road is not achievable.
  - The functionality of any gravity drainage shown is dependent on achieving the minimum allowable gradients. A level survey of the entire route should be carried out prior to construction work commencing on site to confirm this is feasible.
  - Refer to the Architect's drawings for the position of the roof water downpipes and internal foul drainage outlets.
  - Foul drainage discharge shown is subject to final approval from SEPA.
  - Position of any existing services must be accurately located on site prior to construction work commencing.

**LEGEND:-**

	New uPVC foul water pipework (arrowhead denotes direction of flow)
	New uPVC surface water pipework (arrowhead denotes direction of flow)
	New rodding point terminal
	New 450mm uPVC inspection chamber (maximum depth 1.2m)
	New 500mm uPVC polypropylene silt trap (maximum depth 450mm sump (by Wash or equal & approved))

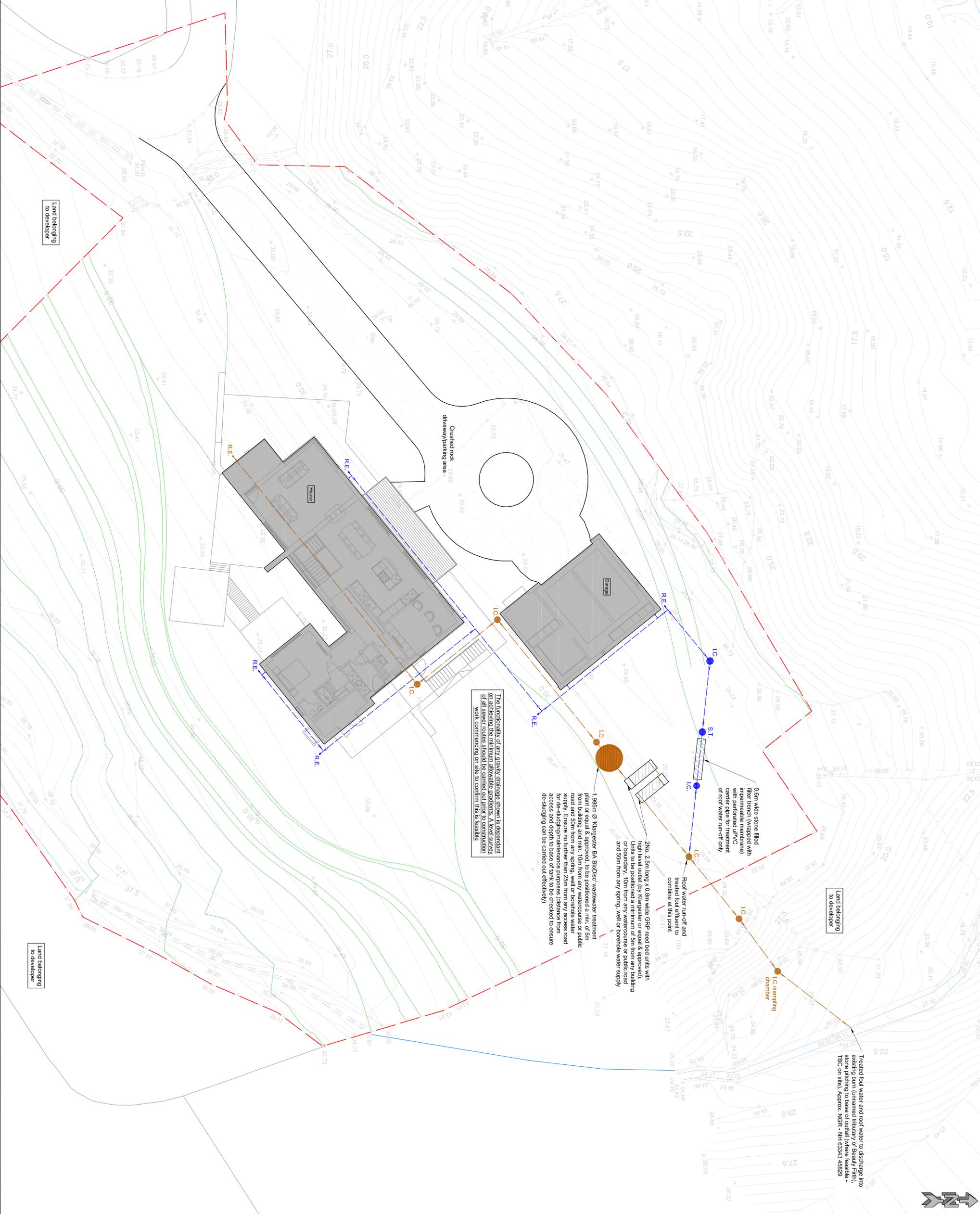
The functionality of any gravity drainage shown is dependent on achieving the minimum allowable gradients. A level survey of all sewer routes should be carried out prior to construction work commencing on site to confirm this is feasible.

1. 995m Ø Kargester BA BioDisc wastewater treatment plant or equal & approved, to be positioned a min. of 5m from building and min. 10m from any watercourse or public road and 50m from any spring, well or borehole water supply. Ensure no further than 25m from any access road for desludging/maintenance purposes (distance from access and depth to base of tank to be checked to ensure desludging can be carried out effectively).

2. No. 2.5m long x 0.8m wide GRP read bed units with high level outlet (by Kargester or equal & approved). Units to be positioned a minimum of 5m from any building or boundary, 10m from any watercourse or public road and 50m from any spring, well or borehole water supply.

3. 0.8m wide stone filled filter trench (wrapped with impermeable membrane) with perforated uPVC down pipe for roof water run-off only.

4. Roof water run-off and treated foul effluent to combine at this point.



Site Layout		New House at Delnore, Inverness	
Drainage Layout Plan			
LAND & BUILDING SURVEYING SETTING OUT ENGINEERS CIVIL ENGINEERING DESIGN LASER SCANNING SERVICES AUTOCAD DRAUGHTING SERVICES HYDROGRAPHIC SURVEYING VOLUMETRIC SURVEYING DIMENSIONAL CONTROL SURVEYING DIMENSIONAL CONTROL SURVEYING DIMENSIONAL CONTROL SURVEYING			
THE OFFICE SUITE 22 USE BECHWOOD BUSINESS PARK INVERNESS IV2 2BN TEL: 01463 811400 TEL: 01463 545000 EMAIL: info@cartech.co.uk WWW: www.cartech.co.uk			
CHIEF: <b>Miss Y. Birrell</b>			
Drawn By:	Checked By:	Survised By:	Survised Date:
KGT	GCN	N/A	N/A
Drawing Status:		Drawing Date:	
<input checked="" type="checkbox"/> PLANNING <input type="checkbox"/> FOR APPROVAL <input type="checkbox"/> WARRANT <input type="checkbox"/> CONSTRUCTION		Drawing Scale: <b>1:125 @ A1</b>	
Levels are in meters and are to Ordnance Datum. (OS Network)		Our Job Ref: <b>J5066</b>	
Drawing No: <b>CTCH-J5066-001</b>		Rev: 	